

CULTURAL RESOURCES ASSESSMENT

OLD TOWNE GATEWAY PROJECT

ASSESSOR'S PARCEL NUMBERS 039-171-12 AND 039-171-03

CITY OF ORANGE

ORANGE COUNTY, CALIFORNIA

LSA

December 2021

CULTURAL RESOURCES ASSESSMENT

OLD TOWNE GATEWAY PROJECT
ASSESSOR'S PARCEL NUMBERS 039-171-12 AND 039-171-03
CITY OF ORANGE
ORANGE COUNTY, CALIFORNIA

Prepared for:

Leason F. Pomeroy, FAIA
LP3 Architecture, Inc.
158 North Glassell Street
Orange, California 92886

Prepared by:

Casey Tibbet, M.A. and Riordan Goodwin, R.A.
LSA Associates, Inc.
1500 Iowa Avenue, Suite 200
Riverside, California 92507
(951) 781-9310

LSA Project No. LPR1702

National Archaeological Data Base Information:

Type of Study: Records Search, Survey, Architectural Evaluation
USGS Quadrangle: Orange, California
Resources Evaluated: 401 West Chapman Avenue (30-159847)
Acreage: ~0.70



December 2021

MANAGEMENT SUMMARY

LSA conducted a cultural resources assessment for the Old Towne Gateway Project located at the northwest corner of West Chapman Avenue and North Cypress Street (Assessor's Parcel Numbers [APNs] 039-171-12 and 039-171-03) in the City and County of Orange, California. The subject property is approximately 0.70 acre and is within the Santa Fe Depot Specific Plan and the Old Towne Orange Historic District. It is developed with three historic-period (50 years of age or older) buildings: a former car dealership building with large additions (401 West Chapman Avenue), a garage (107 North Atchison Street), and a former transformer house with a large addition (135 North Atchison Street). The proposed project includes: demolition of the building at 107 North Atchison Street and the additions to the buildings at 401 West Chapman Avenue and 135 North Atchison Street; rehabilitation of the buildings at 401 West Chapman Avenue and 135 North Atchison Street; and construction of two one-story commercial buildings, a one-story accessory building, an east/west paseo, and three trash enclosures. The City of Orange (City) as Lead Agency for the project required this study as part of the environmental review process to comply with the California Environmental Quality Act (CEQA).

The purpose of the study is to provide the City with the necessary information and analysis to determine, as mandated by CEQA, whether the proposed project would cause substantial adverse changes to any "historical resources" that may exist in or around the project area. In order to identify and evaluate such resources, LSA conducted a historical/archaeological resources records search, pursued historical background research, carried out intensive-level field surveys, and completed impacts analyses.

As a result of these efforts, no archaeological resources were identified within or adjacent to the project area. It was determined that the building at 107 North Atchison Street does not meet the criteria for listing in the National Register of Historic Places (National Register) or the California Register of Historical Resources (California Register) either individually or as a contributor to the Old Towne Orange Historic District. It is not a "historical resource" as defined by CEQA and its demolition will not result in any substantial adverse changes to the Historic District.

The former car dealership building at 401 West Chapman Avenue and the former transformer house at 135 North Atchison Street were evaluated as contributors to the Old Towne Orange Historic District and are, therefore, "historical resources" as defined by CEQA. The proposed rehabilitations of these buildings, as well as the proposed new construction, were analyzed using the *Secretary of the Interior's Standards for the Treatment of Historic Properties* (SOIS), the Historic Preservation Design Standards (HPDS), and the Santa Fe Depot Specific Plan (Specific Plan). In addition, the Depot-Plaza Pedestrian Connection Study (Pedestrian Study) was considered. As a result of these analyses, it was determined that the proposed project is in compliance with the SOIS and the HPDS and meets the objectives of the Specific Plan and Pedestrian Study. However, to provide additional detail and clarity, LSA recommends that the City consider applying the following conditions:

Prior to issuance of building permits:

1. The exterior colors for the Building 1 addition and Buildings 2, 3, 4, and 5 are subject to approval by City staff.

2. To ensure that the alterations to the bay doors are completed in a manner that is compatible with the historic character of Building 1 and the District, it is recommended that they be subject to review and approval by City staff. It is further recommended that both openings have doors/windows that are similar in appearance, design, and materials in order to maintain the current symmetry.
3. Because only the south and east elevations were visible during the field survey, it is recommended that when the Building 4 addition is removed, the openings be verified and, if necessary, the project plans be revised to reflect all of the openings accurately.

In addition, the following standard regulatory compliance measures regarding buried cultural resources are required in conformance with Section 15064.5(e) of the *CEQA Guidelines*, PRC Section 5097.98, and State Health and Safety Code Section 7050.5.

- If buried cultural materials are encountered during earthmoving operations associated with the project, all work in that area should be halted or diverted until a qualified archaeologist can evaluate the nature and significance of the finds.
- In the event human remains are encountered, State Health and Safety Code Section 7050.5 states that no further disturbance shall occur until the County Coroner has made a determination of origin and disposition pursuant to Public Resources Code Section 5097.98. The County Coroner must be notified of the find immediately. If the remains are determined to be Native American, the County Coroner will notify the Native American Heritage Commission (NAHC), which will determine and notify a Most Likely Descendant (MLD). With the permission of the landowner or their authorized representative, the MLD may inspect the site of the discovery. The MLD shall complete the inspection within 48 hours of notification by the NAHC. The MLD will have the opportunity to offer recommendations for the disposition of the remains.

TABLE OF CONTENTS

MANAGEMENT SUMMARY	i
TABLE OF CONTENTS	iii
APPENDICES	iv
FIGURES.....	iv
TABLES.....	v
INTRODUCTION	1
SETTING	4
CURRENT NATURAL SETTING.....	4
Hydrology	4
Biology.....	4
Geology.....	4
CULTURAL SETTING.....	4
Prehistory	4
Ethnography	5
History	6
METHODS	10
RECORDS SEARCH	10
ARCHIVAL RESEARCH	10
ARCHAEOLOGICAL FIELD SURVEY	10
ARCHITECTURAL SURVEY	10
RESULTS.....	11
RECORDS SEARCH	11
ARCHIVAL RESEARCH	11
Project Area History	12
People and Businesses	14
Automotive Dealerships	19
Old Towne Orange Historic District.....	21
Historic Preservation Design Standards	22
Santa Fe Depot Specific Plan	22
Depot-Plaza Pedestrian Connection Study.....	23
Previous Documentation/Evaluations.....	23
ARCHAEOLOGICAL FIELD SURVEY	25
ARCHITECTURAL SURVEY	25
401 West Chapman Avenue	25

107 North Atchison Street.....	30
135 North Atchison Street.....	31
SIGNIFICANCE EVALUATION	34
DEFINITIONS.....	34
National Register of Historic Places (36 CFR Part 60.4).....	34
California Register of Historical Resources (14 CCR 15064.5).....	35
City of Orange.....	36
EVALUATION	36
IMPACTS ASSESSMENT	41
CHARACTER-DEFINING FEATURES	41
PROJECT DESCRIPTION.....	42
PROJECT ANALYSIS.....	45
Standards for Rehabilitation (36 CFR Part 68.3.b)	45
Historic Preservation Design Standards	51
Santa Fe Depot Specific Plan	57
Depot-Plaza Pedestrian Connection Study.....	58
PROJECT ANALYSIS SUMMARY	58
RECOMMENDATIONS	59
REFERENCES	61

APPENDICES

- A: DEPARTMENT OF PARKS AND RECREATION (DPR) 523 FORMS
- B: PROJECT PLANS

FIGURES

Figure 1: Regional and Project Location.....	2
Figure 2: Project Location.....	3
Figure 3: 1922 Sanborn Map	13
Figure 4: 1947 Aerial Photograph (Orange County Archives 2017)	15
Figure 5: 1950 Sanborn Map	16
Figure 6: Dates of Construction Site Estimated Dates of Construction.....	17
Figure 7: 118 West Chapman Avenue (Google December 2020).....	21
Figure 8: 149 North Glassell Street (Google May 2019).....	21
Figure 9: 402–424 West Chapman Avenue (Google December 2020).....	21
Figure 10: 240 West Chapman Avenue (Google December 2020).....	21
Figure 11: 401 West Chapman Avenue, façade/south elevation. View to the north (11/20/18).....	26
Figure 12: 401 West Chapman Avenue, façade and east elevation. View to the northwest (11/20/18).....	26

Figure 13: 401 West Chapman Avenue, east elevation detail. View to the west showing filled in windows (11/20/18). 27

Figure 14: 401 West Chapman Avenue, east elevation, including circa 1940 addition. View to the southwest (11/20/18)..... 27

Figure 15: 401 West Chapman Avenue, rear (north) elevation. View to the south showing the circa 1940 addition (11/20/18)..... 27

Figure 16: 401 West Chapman Avenue, west elevation and partial north elevation. View to the east (11/20/18)..... 28

Figure 17: 403 West Chapman Avenue, north elevation. View to the southwest (11/20/18). 28

Figure 18: 403 West Chapman Avenue, north and west elevations. View to the southeast (11/20/18)..... 29

Figure 19: 403 West Chapman Avenue, south elevation. View to the northwest showing alterations (windows and doors) to this addition (10/31/17). 29

Figure 20: 401–403 West Chapman Avenue, west and south elevations. View to the east (11/20/18)..... 29

Figure 21: 401 West Chapman Avenue, portion of west elevation. View to the southeast (11/20/18)..... 30

Figure 22: 401–403 West Chapman Avenue, overview of west and south elevations. View to the northeast (11/20/18)..... 30

Figure 23: 107 North Atchison Street, west elevation. View to the east (11/20/18). 31

Figure 24: 107 North Atchison Street, south and east elevations. View to the northwest (11/20/18)..... 31

Figure 25: 135 North Atchison Street, façade. View to the east (11/20/18). 32

Figure 26: 135 North Atchison Street, east and north elevations. View to the southwest (11/20/18)..... 33

Figure 27: 135 North Atchison Street, door in east elevation (11/20/18). 33

Figure 28: 135 North Atchison Street, window in north elevation (11/20/18)..... 33

TABLES

Table A: Documented Resources Mapped at the SCCIC 11

Table B: Extant Buildings’ Dates of Construction 18

INTRODUCTION

LSA conducted a cultural resources assessment for the Old Towne Gateway Project, which involves approximately 0.70 acre located at the northwest corner of West Chapman Avenue and North Cypress Street (Assessor's Parcel Numbers [APNs] 039-171-12 and 039-171-03) in the City and County of Orange, California (Figures 1 and 2). The Old Towne Gateway Project area, which is within the Old Towne Orange Historic District and the Santa Fe Depot Specific Plan, is developed with three historic-period (50 years of age or older) buildings: a former car dealership building with large additions (401 West Chapman Avenue), a garage (107 North Atchison Street), and a former transformer house with a large addition (135 North Atchison Street). There is currently a retail store operating out of the former dealership building and the other buildings are being used for storage of miscellaneous items such as cars, car parts, wood, metal barrels, and furniture.

The project proposes to remove the garage (107 North Atchison Street), as well as the additions to the other buildings (401 West Chapman Avenue and 135 North Atchison Street) and rehabilitate the original structures for use as restaurants. In addition, the construction of two one-story commercial buildings, a one-story accessory building, an east/west paseo, and three trash enclosures is proposed. The City of Orange (City), as Lead Agency for the project, required this study as part of the environmental review process to comply with the California Environmental Quality Act (CEQA). As part of that process, the buildings will be evaluated for historical significance under National Register of Historic Properties (National Register) and California Register of Historical Resources (California Register) criteria. In addition, the proposed project will be analyzed for compliance with the *Secretary of the Interior's Standards for the Treatment of Historic Properties* (SOIS) and the City of Orange Historic Preservation Design Standards (HPDS), as well as the objectives of the Santa Fe Depot Specific Plan (Specific Plan) and the Depot-Plaza Pedestrian Connection Study (Pedestrian Study).

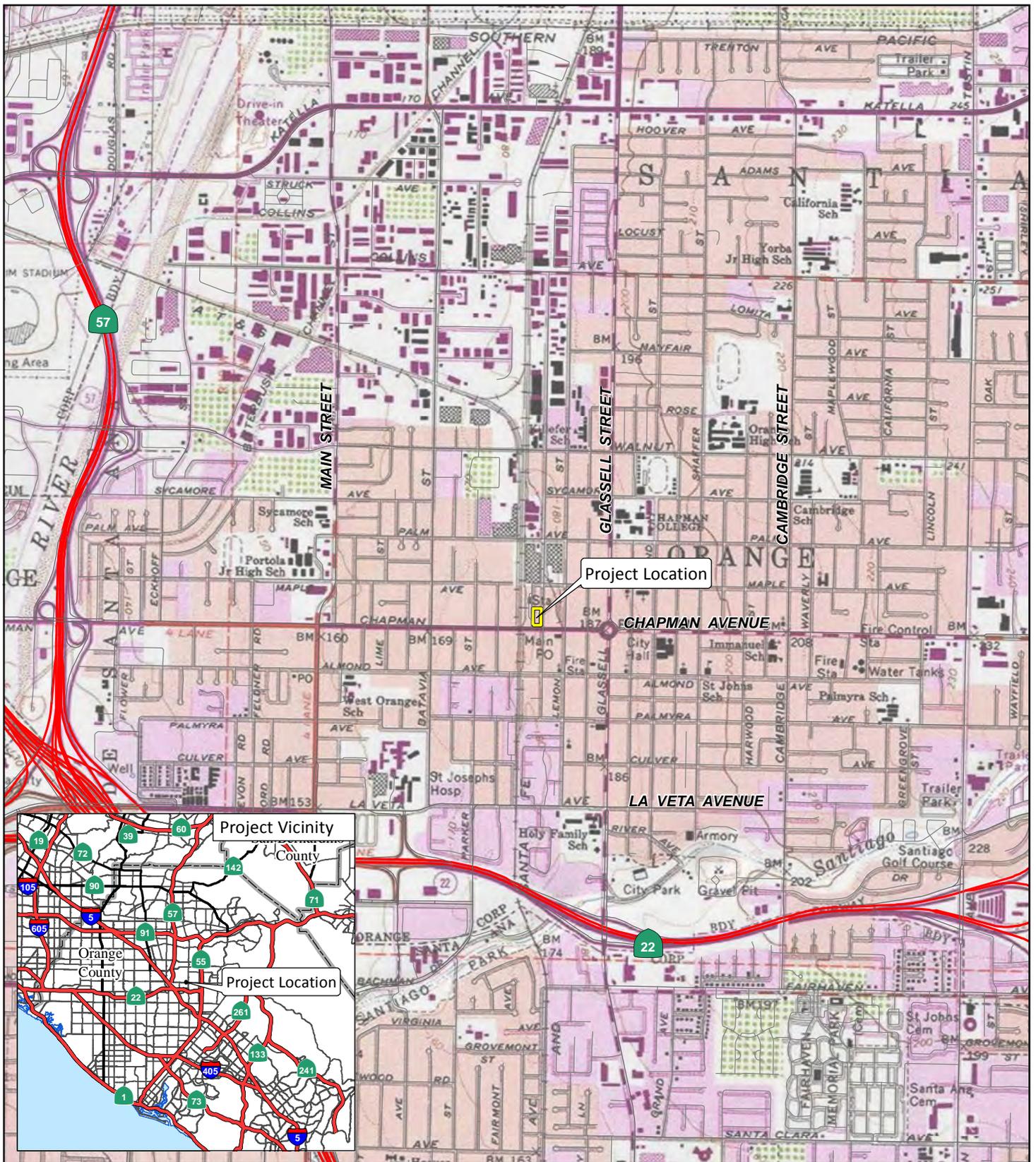
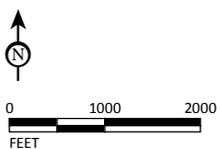


FIGURE 1

LSA



SOURCE: USGS 7.5' Quad - Orange & Anaheim (1981); ESRI Streetmap, 2013.

I:\LPR1702\Reports\Cultural\fig1_RegLoc_05-11-2018.mxd (11/26/2018)

Old Towne Gateway Project
 Cultural Resources Assessment
 Regional and Project Location

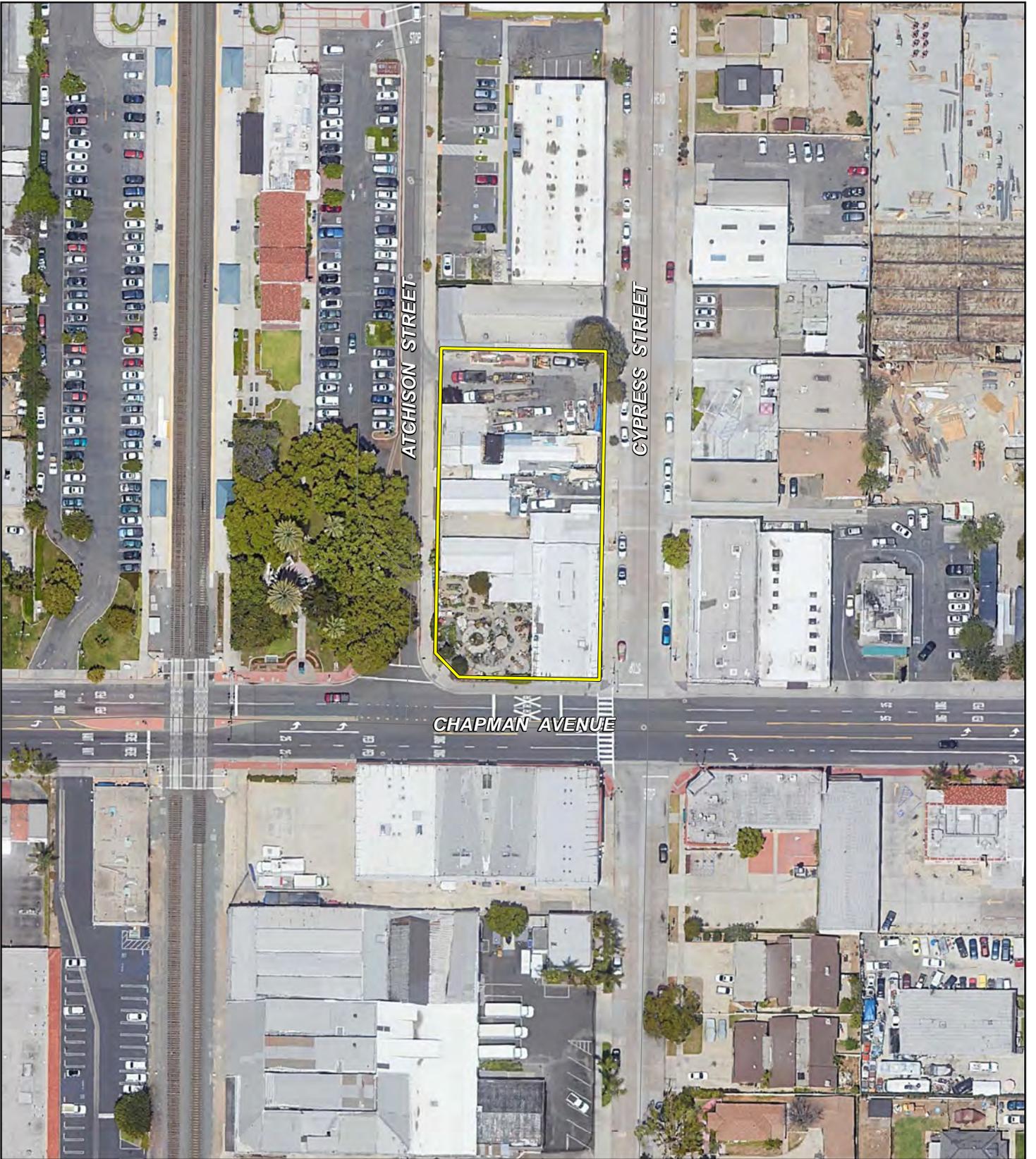


FIGURE 2

LSA

LEGEND

 Project Site



0 50 100
FEET

SOURCE: ESRI Streetmap, 2013; Google Earth, 2018.

I:\LPR1702\Reports\Cultural\fig2_ProjectAreas_05-11-2018.mxd (11/26/2018)

Old Towne Gateway Project
Cultural Resources Assessment
Project Area

SETTING

CURRENT NATURAL SETTING

The project area is within an urban context with mixed-use buildings and supporting infrastructure.

Hydrology

The project region is characterized by a temperate climate, with dry, hot summers and moderate winters. Rainfall ranges from 12 to 16 inches annually (Beck and Haase 1974). Precipitation usually occurs in the form of winter rain, with warm monsoonal showers in summer. The nearest natural source of water is the Santiago Creek (approximately 0.8 mile south), which drains to the west.

Biology

At an elevation of approximately 190 feet above mean sea level (amsl), the project is within the Lower Sonoran Life Zone of California (Schoenherr 1992), which ranges from below sea level to 3,500 feet amsl. Natural vegetation has been removed from the project by development and the only pioneer species noted on the property were xeric grasses. Landscaping vegetation includes oak, sycamore, and fan palm trees as well as various shrubs. Extensive fauna are known locally, including many endemic species of reptiles, birds, and insects.

Geology

The project area is located at the northern end of the Peninsular Ranges Geomorphic Province, a 900-mile-long northwest-southeast trending structural block that extends from the Transverse Ranges to the tip of Baja California and includes the Los Angeles Basin (California Geological Survey 2002; Norris and Webb 1976). The province is approximately 225 miles wide, extending from the Colorado Desert in the east, across the continental shelf to the Southern Channel Islands (Santa Barbara, San Nicolas, Santa Catalina, and San Clemente) in the west (Sharp 1976). This region is characterized by a series of mountain ranges separated by northwest-trending valleys subparallel to faults branching from the San Andreas Fault. The geology of this province is similar to that of the Sierra Nevada, with granitic rock intruding into the older metamorphic rocks.

CULTURAL SETTING

Prehistory

Chronologies of prehistoric cultural change in Southern California area have been attempted numerous times, and several are reviewed in Moratto (2004). No single description is universally accepted as the various chronologies are based primarily on material developments identified by researchers familiar with sites in a particular region and variation exists essentially due to the differences in those items found at the sites. Small differences occur over time and space, which combine to form patterns that are variously interpreted.

Currently, two primary regional culture chronology syntheses are commonly referenced in the archaeological literature. The first, Wallace (1955), describes four cultural horizons or time periods: Horizon I – Early Man (9000–6000 BC), Horizon II – Milling Stone Assemblages (6000–3000 BC),

Horizon III – Intermediate Cultures (3000 BC–AD 500), and Horizon IV – Late Prehistoric Cultures (AD 500–historic contact). This chronology was refined (Wallace 1978) using absolute chronological dates obtained after 1955.

The second cultural chronology (Warren 1968) is based broadly on Southern California prehistoric cultures and was also revised (Warren 1984; Warren and Crabtree 1986). Warren’s (1984) chronology includes five periods in prehistory: Lake Mojave (7000–5000 BC), Pinto (5000–2000 BC), Gypsum (2000 BC–AD 500), Saratoga Springs (AD 500–1200), and Protohistoric (AD 1200–historic contact). Changes in settlement pattern and subsistence focus are viewed as cultural adaptations to a changing environment, which begins with gradual environmental warming in the late Pleistocene, continues with the desiccation of the desert lakes, followed by a brief return to pluvial conditions, and concludes with a general warming and drying trend, with periodic reversals that continue to the present (Warren and Crabtree 1986).

Ethnography

The project area is near the intersection of the traditional cultural territories the Gabrielino (Kroeber 1976; Heizer 1978). The first written accounts of these southern California tribes are attributed to the mission fathers and later documentation was by others indicated below.

Gabrielino

The territory of the Gabrielino included portions of Los Angeles, Orange, and San Bernardino Counties during ethnohistoric times, and also extended inland into northwestern Riverside County (Kroeber 1976; Heizer 1968). It encompassed an extremely diverse environment that included coastal beaches, lagoons and marshes, inland river valleys, foothills, and mountains (Bean and Shipek 1978).

The Gabrielino caught and collected seasonally available food resources, and led a semi-sedentary lifestyle, living in permanent communities along inland watercourses and coastal estuaries. Individuals from these villages took advantage of the varied resources available. Seasonally, as foods became available, native groups moved to temporary camps to collect plant foods such as acorns, buckwheat, chía, berries, and fruits, and to conduct communal rabbit and deer hunts. They also established seasonal camps along the coast and near bays and estuaries to gather shellfish and hunt waterfowl (Hudson 1971).

The Gabrielino lived in small communities, which were the focus of family life. Patrilineally linked, extended families occupied each village (Kroeber 1976; Bean and Smith 1978). Both clans and villages were apparently exogamous, marrying individuals from outside the clan or village (Heizer 1968). Gabrielino villages were politically independent and were administered by a chief, who inherited his position from his father. Shamans guided religious and medical activities, while group hunting or fishing was supervised by individual male specialists (Bean and Smith 1978).

The Gabrielino were described by Johnston (1962), Blackburn (1962–1963), Hudson (1971), and others.

History

The history and development of Orange is well documented and several good summaries of its history have been prepared and are readily available. The following narrative contains consolidated sections of a history authored by notable local/regional historian Phil Brigandi for the Orange Public Library (2011). To read it in its entirety and with accompanying photographs, visit <https://www.cityoforange.org/937/Old-Towne>.

In the 1860s, the vast Rancho Santiago de Santa Ana had been broken up, opening the way for the founding of several new communities. Los Angeles attorneys Alfred Beck Chapman and Andrew Glassell acquired about 9,400 acres of the old Mexican rancho. In 1870 they had several thousand acres near the northern end of the rancho subdivided into large parcels (40, 80, and 160 acres) and placed them on the market. Captain Glassell, Andrew's brother, served as sales agent.

Downtown Orange began in the center of the Plaza [three blocks east of the project area]. In the summer of 1871, Captain William T. Glassell drove a survey stake at the common corners of sections 29, 30, 31, and 32, and laid out a townsite originally known as Richland. In the center, where the two main streets crossed, eight lots were set aside to create a Plaza Square.

When the town of Richland was laid out a year later, several factors determined its location. The area was open and generally level, sloping gradually down towards the confluence of Santiago Creek and the Santa Ana River. The stage road from Los Angeles to San Diego passed not too far west of the townsite. But most importantly, it could be irrigated from the Santa Ana River. In order to develop this site, Chapman and Glassell had to buy another large parcel adjoining their lands. In 1871 an irrigation ditch was dug from the river following the natural contour down to the townsite. (Canal Street, behind The Village of Orange, still marks part of its curving path.) A reservoir was created at the northeast corner of Chapman and Shaffer [seven blocks east of the project area], and iron pipe laid under the street down to the Plaza.

The original Richland townsite was made up of eight city blocks, from Grape Street (now Grand) on the east, to Lemon Street on the west [one block east of the project area]; and from Walnut Avenue (now Maple) on the north to Almond Avenue on the south. Surrounding the townsite were ten-acre plots known as the Richland Farm Lots. The townsite and farm lots covered one square mile.

When the community applied for a post office in 1873, they discovered that there was already a town of Richland in Northern California, so the community was renamed Orange. Local legend says the new name was chosen in a poker game, but in fact, it was chosen for its promotional value. Oranges, and other semi-tropical crops, were becoming identified with Southern California, and there was already talk of forming a separate "Orange County." What's more, the Glassell family had once lived in Orange County, Virginia, on what they called the Richland plantation.

Pioneer Days (1870–1885). The first building in Orange was Captain W.T. Glassell's home and tract office, which stood on the south side of Chapman Avenue, just west of the Plaza. As the 1870s moved on, a smattering of wooden store buildings went up, most of them along Glassell Street. The first two-story building downtown, the Beach Building, was completed in 1874. In

1875, the Plaza Hotel was built of concrete and adobe. By 1885 a small business district had developed, with several general stores, livery stables, and even a newspaper office.

Boom & Bust (1885–1900). In 1886–88, following the arrival of the Santa Fe Railroad, Southern California experienced its most frantic real estate “boom,” and Orange came along for the ride. The first brick building downtown was built in 1885, and several more followed, including the two-story Bank of Orange building (1887), and the three-story Rochester Hotel.

Civic improvement was the order of the day. The Plaza was created in 1886, and the original fountain installed in 1887. The first streetlights went in downtown, and residents could ride streetcars to Santa Ana, Tustin, or El Modena. The railroad reached Orange in 1887 [one block west of the project area], and a year later the city incorporated. Many of the farm lots around downtown were subdivided for residential development, and many new streets were opened up. The names of some of Orange’s best known pioneers are preserved in the tract names—Shaffer, Grote, Harwood, Chubb, Lockwood, Gardner, Beach, Kogler, Cauldwell, and Culver.

But the “boom” was built on speculation, and it collapsed in 1888. Many of the residential lots sold during the boom were later sold for taxes, and most of the subdivisions reverted to agricultural land. About this same time, a mysterious disease (now known to be *aphylloxera*) destroyed most of the vineyards that had been the backbone of the local economy. More and more ranchers began to plant oranges, but it would be several years before the trees matured and the local economy revived.

Growing Up (1900–1920). Orange’s economy expanded rapidly in the early 20th century, and downtown grew with it. Most of the landmark buildings around the Plaza were built during this period, and residential construction increased, spreading further and further out from the center of town. Instead of single store buildings, downtown businessmen and investors built “blocks” of connected storefronts, with the upper floors often reserved for apartments or meeting rooms.

As downtown Orange grew up, residents no longer needed to go to Santa Ana or Anaheim for major shopping. Saturday nights, the streets around the Plaza would be crowded with people, doing their shopping for the week. By the end of the First World War, most of the land around downtown Orange was subdivided for residential neighborhoods.

Growing Out (1920–1950). After World War I, businesses began moving further and further west from downtown. State Highway 101 came down West Chapman as far as Main Street, before turning south toward Santa Ana [approximately one-half mile west of the project area]. A little business district developed at the corner. Since it was midway between Orange and Santa Ana, it was dubbed “Orana.”

Orange also began to develop an industrial strip along either side of the Santa Fe railroad tracks [one block west of the project area]. Local packing houses had always been close to the railroad, but now they were joined by several manufacturing plants, most notably Anaconda Wire & Cable [at 200 North Cypress Street one block north of the project area].

In the late 1920s, Orange’s first Planning Commission proposed that all of downtown should be done over in the then-popular Mission Revival style. The buildings on the south side of the first block of East Chapman Avenue were remodeled in that style in 1928, complete with red tile and stucco arches, but the coming of the Depression put an end to that project.

Residential development continued in the downtown area. New homes were built, filling in the vacant lots on many blocks, and the last few downtown subdivisions were laid out in the 1920s.

Suburbanization and Decline (1950–1970). After World War II, Southern California began to grow rapidly, and Orange came along for the ride. New retail areas developed, most notably along Tustin Avenue. In the early 1970s, both the Mall of Orange (now called The Village at Orange) and The City Shopping Centre (now the site of The Outlets at Orange) opened. All of these developments drew businesses away from downtown.

In the 1950s, the idea of transforming the Plaza area into a pedestrian mall was first floated, and was widely debated on into the 1960s. In 1965 the City Council went so far as to authorize a feasibility study for a Plaza Mall plan. The idea was still being talked about in 1967, when two young architects proposed a 10-block “Super Plaza” with high-rise apartments all around downtown.

Residential development also moved out away from downtown, as many areas that had once been orange groves or farms were subdivided. By the mid-1950s, the first large-scale tract home developments were being built in Orange, and the City began annexing more and more of these outlying areas. Orange’s population grew from just 10,000 in 1950 to over 77,000 in 1970.

As the City’s boundaries and population grew, new civic and institutional buildings such as schools, libraries, fire stations, and churches were constructed and many of the older ones were enlarged. [In 1954, Chapman College, now Chapman University, moved to Orange taking over the site formerly occupied by Orange High School (Chapman University 2019). Throughout its history in Orange, it has played a key role in the revitalization and success of the Old Towne area. There are university facilities, including the Hilbert Museum of California Art, the West Campus parking structure, and Dodge College, within one block of the project area.]

Rebirth (1970–Present). The Plaza mall idea had its last gasp in 1969. That same year, Mayor Don E. Smith proposed a “revitalization” of downtown. Not just the Plaza, but the surrounding streets as well. First on the agenda was the Plaza Square. In 1970 the old palm trees in the corners were removed, the streetlights replaced, and new brick sidewalks and planters installed. Phase Two called for moving out onto Chapman Avenue and Glassell Street, but the cost of the Plaza work was higher than expected, and the City Council voted not to spend any more money on the revitalization project.

Major retailers continued to abandon downtown in the early 1970s. In their place, antique stores began to fill in the old storefronts, and by the 1980s they were the major commercial force around the Plaza. In more recent years, they have been joined by more restaurants and cafés, and other businesses. During this same era, people began discovering the downtown residential neighborhoods. By the mid-1970s, historic homes began to rise in price as more and more young families abandoned tract housing to live in the bungalows and Mediterraneans of old downtown Orange.

In the late 1970s, the plaza idea was revived as a historic preservation project for the area, and in 1979 the City formed an Old Towne Steering Committee to develop a plan for the future of downtown Orange. It was decided to continue the brick sidewalks of 1970 out onto the

spoke streets, adding specially designed street furniture. The work on the new streetscapes for North and South Glassell was done in 1983. Matching brickwork on East and West Chapman followed in 1985.

Revitalization of downtown Orange continues to the current date with the introduction of restaurants, commercial services and specialty retail uses, often rehabilitating and occupying historic structures (e.g., Urth Caffè at 100 W. Chapman Avenue). The proposed project is a continuation of that revitalization and includes a critical piece of a long-desired pedestrian connection between the Santa Fe Depot and the Plaza.

METHODS

RECORDS SEARCH

On April 12, 2018, LSA Senior Archaeologist Virginia Austerman conducted a cultural resources records search for the project area at the South Central Coastal Information Center (SCCIC) at California State University Fullerton. The SCCIC houses the pertinent archaeological site and survey information necessary to determine whether cultural resources are recorded within the project area boundaries and which specific areas have been previously surveyed. The research included a review of all recorded historic and prehistoric archaeological sites within one mile of the project, as well as a review of known cultural resource survey and excavation reports. In addition, LSA examined the California State Historic Property Data File (HPD), which includes the National Register of Historic Places (National Register), California Historical Landmarks (CHL), California Register of Historical Resources (California Register), and California Points of Historical Interest (CPHI), various local historic registers, and historic maps.

ARCHIVAL RESEARCH

LSA completed archival research between November 2017 and October 2021. Research methodology focused on the review of a variety of primary and secondary source materials relating to the history and development of the project area. Sources included, but were not limited to, building permits, online sources, published literature in local and regional history, news articles, city directories, historic aerial photographs, and historic maps, including Sanborn Fire Insurance maps (Sanborn). The City of Orange and the Orange County Archives were also contacted.

ARCHAEOLOGICAL FIELD SURVEY

On April 15, 2018, LSA Senior Archaeologist Virginia Austerman visited the project area. The purpose of this survey was to identify and document—prior to the beginning of ground-disturbing activities—any cultural resources and thus also to identify any area(s) that might be sensitive for buried cultural resources.

ARCHITECTURAL SURVEY

On October 31, 2017, LSA Architectural Historian Casey Tibbet conducted the intensive-level architectural survey of the project area. During the survey, Ms. Tibbet took numerous photographs of the exteriors of the buildings and their setting. In addition, she made detailed notations regarding the structural and architectural characteristics and current conditions of the buildings and associated features. The purpose of this survey was to attempt to obtain views of each side of the building. Ms. Tibbet conducted a follow-up field survey on November 20, 2018.

RESULTS

RECORDS SEARCH

Data from the SCCIC indicate that there have been 24 previous cultural resource studies conducted in the records search area, none of which included any portion of the project. Thirteen cultural resources have been mapped within the one-mile radius of the project, none of which is located within the project parcels. Of the 13 resources, two are historic districts (30-158679 and 30-159932), which consist of over 1,300 properties, many of which are listed in the National Register, one is a railroad alignment, and 10 are historic-period buildings, features, and transportation infrastructure. No prehistoric resources have been documented within one mile of the project.

The Old Towne Orange Historic District (30-159932) was added to the National Register in 1997 and encompasses a one-mile square area that contains over 1,300 homes and other types of buildings. The structures within the District represent more than 50 different architectural styles and exemplify life in Orange between the years of 1888 and 1940. The Plaza Historic District (30-158679) includes 53 buildings, 35 of which are listed in the National Register.

Table A provides the list of cultural resources in the records search that are documented and mapped at the SCCIC.

Table A: Documented Resources Mapped at the SCCIC

Primary #	Site Description
30-158658	172 N. Glassell Street; Pantages/Orange Theater
30-158679	The Plaza Historic District; 53 buildings, 35 of which are National Register listed
30-158680	Intersection of Chapman Avenue/Glassell Street; The Orange Plaza developed in 1886
30-158686	333 N. Glassell Street; Chapman College; built in 1913
30-158759	163 South Cypress Street; Parker House built in 1887
30-158935	185 South Center Street; St. John's Lutheran Church built in 1913
30-159075	192 South Orange Street; First Baptist Church of Orange; built in 1893 and National Register listed
30-159124	205 E. Palmyra Avenue; C.Z. Culver House built in the 1880s and National Register listed
30-159820	414 East Chapman Avenue; Lewis Ainsworth House built in 1910
30-159886	370 North Glassell Street; Orange Intermediate School/Central Grammar School; National Register listed
30-159932	Old Town Orange Historic District; 1,300+ properties listed in the National Register
30-176535	189 South Water Street; Water Department Pump House Building
30-176663	Burlington Northern and Santa Fe Railway

ARCHIVAL RESEARCH

This section discusses the history of the project area and people associated with it and also provides a context for automotive dealerships, including related evaluation criteria, information about the Old Towne Orange Historic District (Historic District), the Historic Preservation Design Standards (HPDS), the Santa Fe Depot Specific Plan (Specific Plan), and the Depot-Plaza Pedestrian Connection Study

(Pedestrian Study). Information about previous documentation and/or evaluations of the buildings within the project area is also provided.

Project Area History

No original building permits were found for any of the buildings in the project area. In order to determine the history of the property, a variety of sources were reviewed including, but not limited to, Sanborn Fire Insurance Maps, historic aerial photographs, newspapers, city directories, and building permits for later alterations.

Sanborn maps from 1909 do not show any of the existing buildings on the property (Sanborn Fire Insurance Maps 1909). In 1912, North Atchison Street on the west side of the project area was paved and research indicates that the small Mission Revival style building (now 135 North Atchison Street) extant in the project area was constructed about that time as a transformer house (*Orange Daily News* 1912; Brigandi 2017). This building is shown in the project area at 125 Atchison Street on the 1922 Sanborn maps (Figure 3; Sanborn Fire Insurance Maps 1922). At that time, there were eight other buildings in the project area, none extant, and surrounding properties were developed with lumber yards (north and south), packing houses and a railroad depot (west), and residences (Ibid.).

In 1920, Pease, Kolberg & Company, successor to Pease Seed and Grain Company, began operating a planing mill and lumber yard at its property on the southwest corner of West Chapman Avenue and South Cypress Avenue, across Chapman Avenue from the project area (*Santa Ana Register* 1920a). That same year, the company became the southern California and Arizona distributors for Clydesdale trucks (*Santa Ana Register* 1920a). News articles from 1923 announced that A.H. Pease of Pease, Kolberg & Company planned to build two buildings both on West Chapman Avenue near Cypress (*Santa Ana Register* 1923). The building planned for the Citrus Tire and Truck Company was proposed to be built on the site then occupied by the Pease, Kolberg, & Company planing mill (across Chapman Avenue from the project area) and a building planned for Harry D. Riley, automobile agent, would be at 401 West Chapman Avenue (in the project area; Ibid.). The building in the project area was to be “single story with a high ceiling adding an air of distinction to the place. The structure, which will be forty-six by eighty-two feet, will contain tile floors with marble base, mezzanine floor, offices, rest rooms and an attractive showroom rivaling anything of its kind in the county” (Ibid.). Work on the two projects was slated to start simultaneously as soon as the plans were accepted (Ibid.).

In January 1924, it was reported that, in 1923, a “block of six storerooms by the Pease-Kolberg Company, at a cost of \$100,000” was constructed on West Chapman Avenue across the street from the project area (*Los Angeles Times* 1924a). In March 1924, another article indicated that Pease-Kolberg was “erecting a beautiful building, which will be the home of the Studebaker people. The building will have a frontage of forty feet on Chapman and eighty-six feet on Cypress Street. There will be an upstairs where the offices and a luxurious woman’s rest room will be” (*Los Angeles Times* 1924b). This was the building at 401 West Chapman Avenue. When constructed, the building was actually a bit larger (50 feet by 100 feet) than the news articles had reported (LP3 Architecture, Inc. 2020).

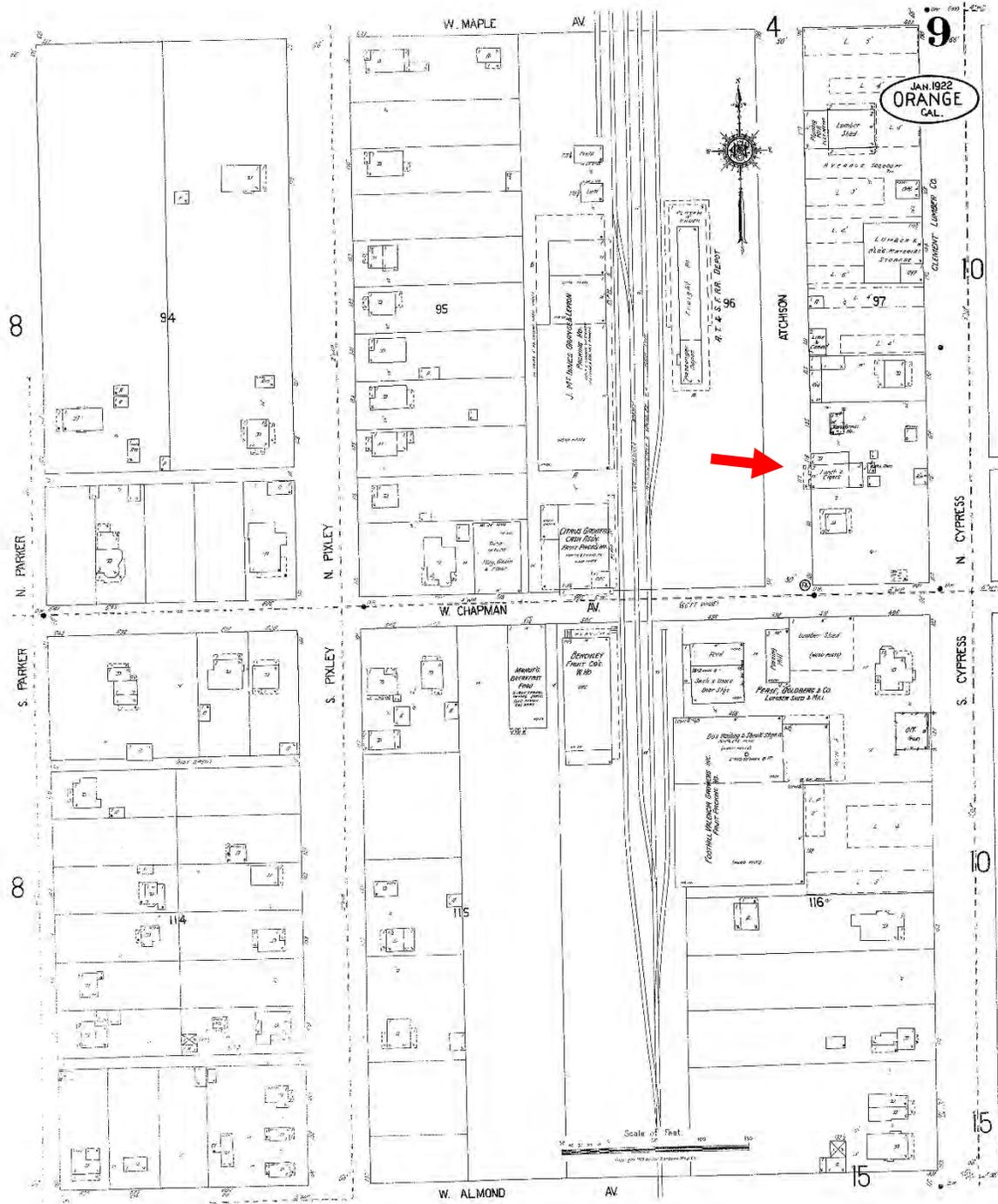


Figure 3: 1922 Sanborn Map

The 1924 City Directory has a listing for Harry D. Riley Studebaker automobiles at 400 W. Chapman Avenue in Orange, but in 1925 his business is listed at 404 W. Chapman Avenue, and in 1928 news advertisements place the business at 401 W. Chapman Avenue (Ancestry.com var.; *Santa Ana Register* 1928a). News articles also mention Schwab Garage and a resident at 401 West Chapman Avenue in 1927 and Sackett and Doty Garage there in April 1928 (*Santa Ana Register* 1927a, 1927b, and 1928b).

A 1947 aerial photograph and a 1950 Sanborn map show that the project area included:

- An auto sales and repair building with concrete floors, wood truss roof, and skylight, which is the dealership building (401 and 405 West Chapman Avenue);
- An auto repair building with iron-on-frame construction and a concrete floor (attached to the west elevation of the dealership building);
- A patio cover (attached to the west elevation of the dealership and the south elevation of the repair building);
- A very small store in the middle of the used auto sales lot at the corner of Chapman Avenue and Atchison Street (411 West Chapman Avenue);
- An auto repair building (118 North Cypress Avenue) with iron-on-frame construction and a concrete floor (attached to the north end of the dealership building, constructed between 1938 and 1947);
- A shade structure for car washing (no address);
- A one-story iron-on-frame storage building that wraps around the west and north sides of the Mission Revival style transformer house, which is labeled as vacant (125 Atchison Street); and
- An L-shaped machine shop with iron-on-frame construction and a concrete floor (136 North Cypress Avenue, no longer extant) (Figures 4 and 5; Sanborn Fire Insurance Maps 1950; Orange County Archives 2017).

The earliest building permits found for 401 West Chapman Avenue date to 1993 and are for exterior alterations and seismic retrofit (City of Orange var.). In 1999, permits were issued for a sign and to replace the existing brick façade with a similar product (Ibid.). In 2009, a permit was issued to “repair car damaged wall front corner of building facing Cypress. Like for like” (Ibid.). In 2011, a sign permit was issued (Ibid.).

Figure 6 and Table B provide dates of construction for the extant buildings.

People and Businesses

401 West Chapman Avenue

In 1919, Harry D. Riley of San Diego purchased the T.L. Longworth garage in Anaheim (*Santa Ana Register* 1919). Riley was going to be the northern Orange County agent for Studebaker and had been the Ford agent in San Diego for six-and-a-half years (Ibid.). By 1920, Riley was a member of the Board of Trade, a city advisory committee, and was advocating for advertising the City of Anaheim (*Santa Ana Register* 1920b). Riley was named the first president of the Anaheim Chamber of Commerce and held the position



Figure 4: 1947 Aerial Photograph (Orange County Archives 2017)

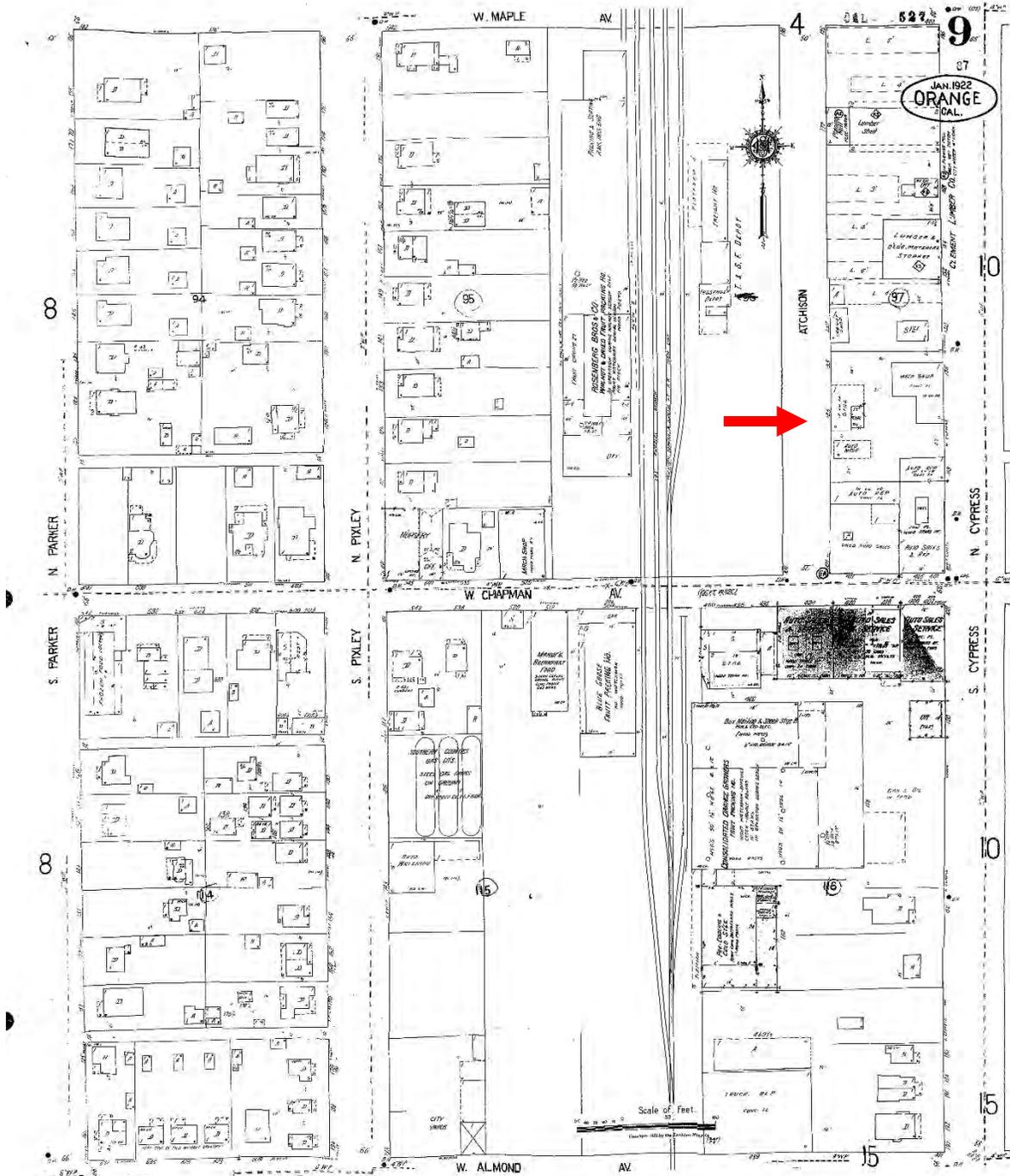
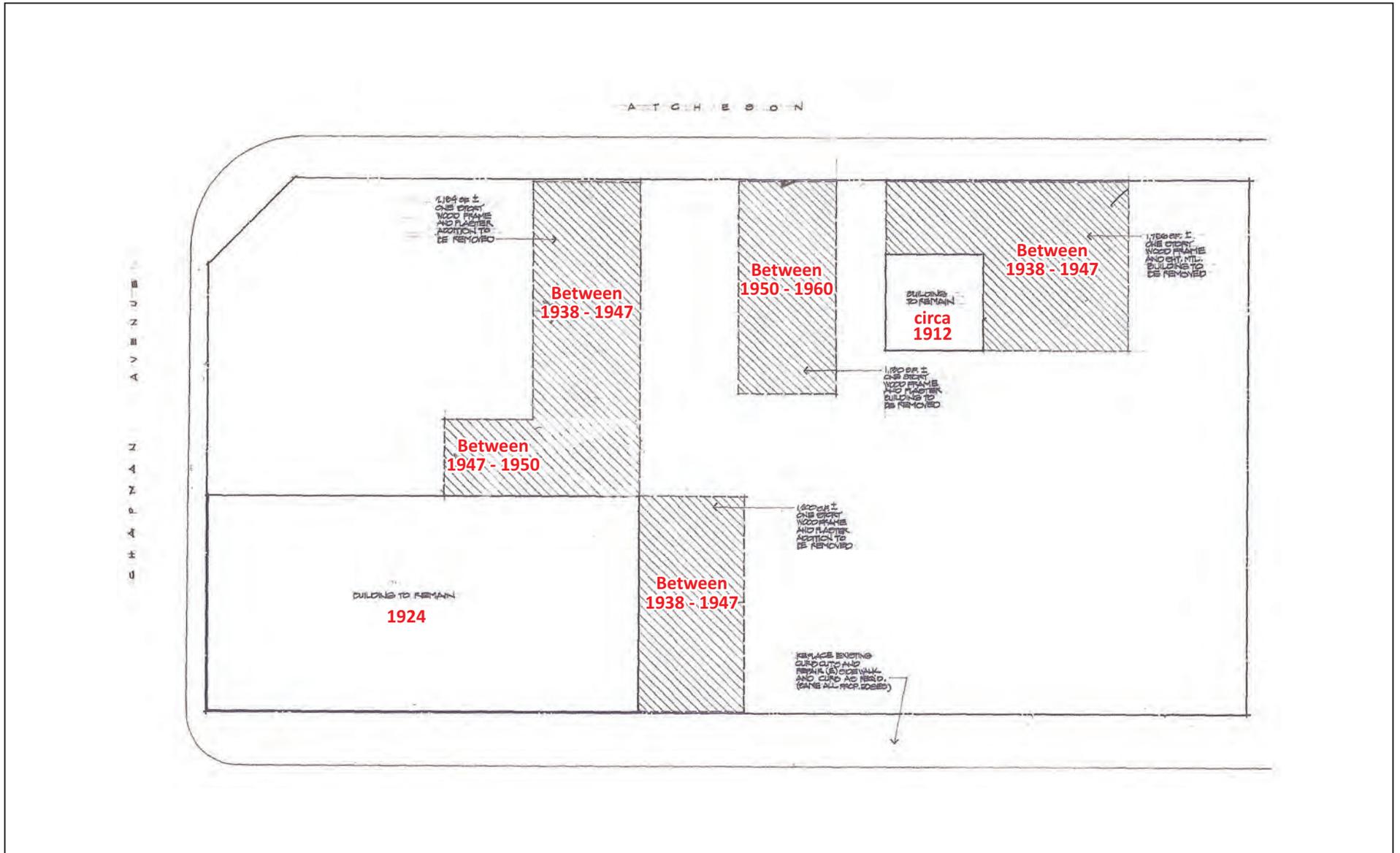


Figure 5: 1950 Sanborn Map



LSA

FIGURE 6



Old Towne Gateway Project
 Estimated Dates of Construction

Table B: Extant Buildings' Dates of Construction

Building	Year Built	Source
Mission Revival style building (135 N. Atchison Street)	Circa 1912	Brigandi 2017 <i>Orange Daily News</i> 1912
L-shaped addition to the north and west sides of the Mission Revival style building	Between 1938 and 1947	Sanborn Maps (1922 and 1950) Historic aerial photographs (1938 and 1947)
Dealership building (401 W. Chapman Avenue)	1924	<i>Los Angeles Times</i> 1924b
Rectangular addition to the north end of the dealership building	Between 1938 and 1947	Sanborn Maps (1922 and 1950) Historic aerial photographs (1938 and 1947)
East/west-oriented addition to the west side of the dealership building	Between 1938 and 1947 (1947 free-standing, 1950 attached to the dealership)	Sanborn Maps (1922 and 1950) Historic aerial photographs (1938 and 1947)
Small addition attached to the west side of the dealership	Between 1947 and 1950	Sanborn Map (1950) Historic aerial photographs (1938 and 1947)
Free-standing building (107 N. Atchison Street)	Between 1950 and 1960	Sanborn Map (1950) Historic aerial photographs (1955 and 1960)

from 1920–1925; it was during this time he led the fight against the Ku Klux Klan in Anaheim (Brigandi 2017; *Santa Ana Register* 1920c). Research indicates that the petition circulated to recall the councilmen with ties to the Klan was actually sponsored by the U.S.A. Club, but the Chamber of Commerce led by Harry D. Riley did join in the fight (*Los Angeles Times* 1925a and 1925b). In January 1925, just weeks before the recall vote, the Anaheim Chamber published a resolution stating in part “Whereas, the present situation has developed beyond any political issue into a community crisis involving the entire future of our community ... the facts are clear that the Ku Klux Klan has not only gained control of the political affairs of our city, but has become a menace alike to the peace and business prosperity of our city ... and it is not only impossible to get industries to locate here under its domination, but it has become a question of holding several of our present leading industries ... therefore, be it resolved by this board of directors that we and the entire Chamber of Commerce throw our influence and efforts into the campaign to rid the city of this Ku Klux Klan control ... signed by Harry D. Riley, president” and others (Ibid.). After the February 3, 1925, vote, the councilmen were recalled and replaced with anti-Klan candidates, as were their Klan-friendly appointees (Brigandi 2017; *Los Angeles Times* 1925c). Although this did not end all Klan activity, it was a decisive blow to the organization in north Orange County (Brigandi 2017).

During the 1920s, Mr. Riley lived in Anaheim and had at least three dealerships in Orange County (Ancestry.com var.). In 1928, Riley built a new garage at 503 South Main Street in Santa Ana and, in summer 1929, it was announced that the Riley family had moved from Anaheim to Santa Ana (*Santa Ana Register* 1929a and 1929b). A subsequent article reported that Mr. Riley had plans to expand his Santa Ana facility and would be concentrating his businesses and civic interests in that city since the leases on his buildings in Anaheim and Orange had expired in January 1929 (*Santa Ana Register* 1929c).

After Riley’s departure at the end of 1928, the building at 401 West Chapman Avenue was occupied by various auto-related businesses until at least 1966 (Chattel 2005a; *Los Angeles Times* 1966). A

combination of City Directory listings and news advertisements revealed the following historic-period occupants at 401 West Chapman Avenue: Schwab Chevrolet Co. (1928), Mansur Motor Co. (Ford dealer, 1937–1953), and Selman Chevrolet (early 1966) (Orange County Archives var.; *Los Angeles Times* 1966). In addition, previous documentation for the building lists Sackett-Doty Chevrolet and Doty Chevrolet as former occupants in the late 1920s and early 1930s (Chattel 2005a). In 1930, Doty bought out Schwab, but the 1932 City Directory lists Doty Chevrolet at 402 West Chapman Avenue as does a *Los Angeles Times* advertisement from 1934 (Orange County Archives var.; *Santa Ana Register* 1930). This location was repeated in numerous advertisements in 1934 and one in 1936. Advertisements put Clift's Marine Shop at 403 West Chapman Avenue (west addition to 401) in 1959 and Speed & Marine Associates at 401 West Chapman Avenue in 1966 through at least 1969 (*Los Angeles Times* 1959, 1966, and 1969).

107 North Atchison Street

This building was constructed sometime after 1950 and appears to have replaced a free-standing canopy that was used as an auto wash (Sanborn Fire Insurance Map var.). No information about the owners/occupants during the historic period was found. This address was not found in the available City Directories and an online newspaper search using the address did not find any matches.

135 North Atchison Street

In 1922 and 1950, this building's address was 125 North Atchison Street. A search of City Directories using 125 and 135 North Atchison Street revealed three listings for the building between the early 1920s and mid-1960s. In 1924, the occupant was Orange County Renovators and in 1925 and 1965 it was listed as vacant (Orange County Archives var.). Online newspaper searches for both addresses did not return any matches. Previous documentation for this building indicates that in 1927–1928, the building was used as a repair shop for Schwab Chevrolet and that it continued to service subsequent dealerships at 401 and/or 403 West Chapman Avenue into the early 1990s (Chattel 2005a). No additional information regarding historic-period owners/occupants was found.

Automotive Dealerships

From 1897 through World War II, the proliferation of the automobile had a profound impact on American culture and the built environment. Automobiles “required major changes in the infrastructure such as the improvement of roads, the construction of bridges, and the development of the highway network. This new, improved, and automobile-oriented infrastructure transformed the landscape and facilitate the decentralization of cities ... [and] reshaped the built environment by requiring modifications to existing structures and necessitating the development of new building types such as gas stations and automobile showrooms” (Historic Resources Group 1996:3).

Until the early 1920s, car sales and services were often incorporated into existing businesses, such as livery stables, blacksmith shops, or machine shops, but as cars became increasingly popular, the related businesses became more specialized. For example, in 1919, about 47 percent of the gas sold was by grocery stores, general stores, and hardware stores, but by the end of the 1920s these outlets had virtually disappeared, having been replaced by free standing gas stations (Margolies 1993:44). Similarly, while many auto dealers “continued to service cars they sold, the functions of shipping, storing and repairing cars were increasingly performed by other businesses specifically created to meet these needs.

It was at this point that the automobile show and sales room came into its own” (Historic Resources Group 1996:8). Dealerships proliferated in the mid-1920s, but began to decline by the end of the 1920s as auto companies consolidated or folded due to competition (Ibid.). By the 1930s, car sales were falling and dealers added repair services to help make ends meet (Ibid.). During the war years, production came to a halt delivering another blow to the dealers (Ibid.).

This pattern does not seem to have played out in Orange, as research found that the number of dealerships held steady at five from 1922 to 1928 (Orange County Archives var.). Between 1932 and 1950, the numbers went up and down from a low of two (in 1932 and 1941) to high of five (in 1937 and 1950) (Orange County Archive var.; Sanborn Fire Insurance Map 1950). Reviews of the 1922 and 1950 Sanborn maps show similar results for automotive-related businesses. In 1922, a total of 20 businesses related to cars, trucks, and motorcycles were identified in the 20-block area bound by Parker, Maple, Shaffer, and Almond. The majority of the businesses were repair shops, but there were three auto sales businesses at 102–106 West Chapman Avenue, 149 North Glassell Street, and 123 South Orange Street (Sanborn Fire Insurance Map 1922). There was also one repair business at 114–120 West Chapman Avenue that included “20 cars” indicating that there may have been a sales component to the business (Sanborn Fire Insurance Map 1922).

By 1950, Sanborn maps show a total of 23 automotive businesses in the same 20-block area and five of those were auto sales businesses, all at different locations than the 1922 businesses. Four of the 1950s businesses were on the northwest and southwest corners of West Chapman Avenue and Cypress Street (this includes the project area) and the other was at 190 North Orange Street. Only the multi-unit building at the southwest corner of Chapman Avenue and Cypress Street (402–424 West Chapman Avenue) still houses an automotive-related business. Buildings in the 20-block area that retain some features, such as large showroom windows, from their former uses as dealerships include 118 West Chapman Avenue, 240 West Chapman Avenue, 401 West Chapman Avenue (project area), 402–424 West Chapman Avenue, and 149 North Glassell Street (Figures 7 through 10). All of these buildings appear to date to the 1920s and all have sustained alterations to facilitate new uses. With the exception of 240 West Chapman Avenue, all of the buildings are contributors to the Old Towne Orange Historic District.

In the post-WWII period, “cheap fuel, mass-produced automobiles, and urban expressways contributed to increasingly decentralized cities” (Historic Resources Groups 1996:17). It was during this period that the huge auto centers were developed. The first auto center in the nation was built in 1965 in Riverside, California when seven of the area’s most successful auto dealerships came together with the concept of showcasing their inventories in a common setting (Riverside Art Museum n.d.). This concept, which was revolutionary in 1958 when the Riverside dealers first conceived of it, is now the norm throughout the country (Ibid.). The City of Orange, however, continues to have several smaller dealerships scattered throughout the city rather than a large auto center.

Evaluation Criteria

As a property type, pre-WWII auto dealership showrooms were typically located on main thoroughfares and reflected the Period Revival styles popular at the time. They were usually one or two-stories of masonry construction with wood or steel truss roofs with showrooms in the front and



Figure 7: 118 West Chapman Avenue (Google December 2020).



Figure 8: 149 North Glassell Street (Google May 2019)



Figure 9: 402-424 West Chapman Avenue (Google December 2020)



Figure 10: 240 West Chapman Avenue (Google December 2020).

service facilities in the rear accessed by alleys or side streets (Historic Resources Group 1996). “Architectural styling and ornamentation was concentrated on the showroom portion, particularly around the formal entrances. Expansive plate glass windows were also a dominant feature of the primary façade. Exterior cladding such as concrete and stucco was often scored, carved or cast to create the appearance of stone. High ceilings, waiting rooms and decorative floors distinguished the interior showroom spaces. ... The interiors of the service facilities were characterized by bare concrete floors, exposed roof trusses and skylights” (Historic Resources Group 1996:19).

Automobile showrooms may be individually significant if they retain a high degree of architectural integrity identified with the original use and they have a strong association with a particular dealership or automotive company (Criteria A/1); are associated with an individual who played a leadership role in the automotive sales business in Orange, operated their business at this location for a significant period of time, and for whom there are no other extant associated buildings (Criteria B/2); or is an excellent example of an architectural style or architect’s work with few or no alterations (Criteria C/3; Historic Resources Group 1996). This property type may also be significant as a contributor to a historic district if located along a historic auto row or within a cluster of showrooms, as long as sufficient integrity from the period of significance remains (Ibid.).

Old Towne Orange Historic District

The District (30-159932) was listed in the National Register in 1997 (California Office of Historic Preservation 2020).

[The District] “includes more than 1,300 homes and other buildings. It is approximately one square mile in size. The district provides a feeling for life in Orange from 1888 to 1940, showcasing over 50 different architectural styles. The complete stock of buildings, which are a part of the Old Towne community, is complemented by the churches, schools, old Santa Fe Depot, Post Office, packing houses, industrial buildings, clubhouses, and parks, which still remain in active use since their establishment in the early part of the century.

To build upon the documentation of properties in the Old Towne Orange National Register District nomination, the City surveyed all pre-1940 buildings in 1982 (updated in 1992) and established expanded Local Historic District boundaries that include properties on the periphery of the National Register District in recognition of the fact that while these areas may not meet the National Park Service criteria for National Register designation, they bear a relationship to the National Register District and remain special areas of historic importance in Orange that warrant preservation and conservation” (SeeCalifornia.com n.d.).

The District is significant under National Register Criteria A and C and is associated with the themes of transportation, commerce, industry, agriculture, architecture, and exploration/settlement. The period of significance for the District is 1888 to 1940. The project area is within the boundaries of the National Register-listed District.

Historic Preservation Design Standards

The HPDS include standards for historic commercial buildings along spoke streets such as Chapman Avenue, additions to historic buildings, and new infill construction within the District. According to the HPDS, “the HPDS ensures that proposed new construction is compatible with existing historic features through a range of flexible design guidelines” (City of Orange 2018:1). The HPDS states that “successful infill construction takes cues from the surrounding historic neighborhood and its buildings” but does not replicate historic styles (City of Orange 2018:46). According to the HPDS, infill buildings should respond to their context within a historic neighborhood. They should be similar to the historic-period buildings in terms of setbacks, scale, massing, height, roof forms, materials, and patterns of windows and doors. New buildings do not necessarily have to meet all of the guidelines, but should incorporate enough of them to be compatible with the District while still clearly modern.

Santa Fe Depot Specific Plan

The project area is within the Specific Plan area, which is located at the crossroads of the Santa Fe Railroad tracks and Chapman Avenue just west of the project area (Arroyo Group 2012). Prior to the 1920s, the Specific Plan area was dominated by packing houses and industrial facilities, as well as modest houses located along Pixley, Cypress, and Lemon Streets that provided homes for the workers (Ibid.). After World War I, the area transitioned to commercial uses related to the automotive industry (Ibid.). New one-story buildings with large “display windows and glazed entrance doors set into a façade veneered with decorative brick and terracotta” went up along Chapman Avenue (Arroyo Group 2012:12). By World War II, the mixed-use character of the area had solidified with a mix of industrial, commercial, and residential structures (Ibid.). One of the stated objectives of the Specific Plan is to “Maintain and enhance the historic industrial character of the Santa Fe Depot area, as well as the remaining integrity of historic residential areas, through careful and coordinated planning that builds upon its key assets and reinforces its historic development patterns” (Arroyo Group 2012:23). Another

purpose of the Specific Plan is to capitalize on pedestrian traffic by designing development “for pedestrians, with pedestrian linkages to the transportation center and connections to the Plaza, Chapman University, residential neighborhoods, and other destinations in the area. The intent is to bring new vitality to the Santa Fe Depot area, making it a vibrant part of Old Towne Orange” (Arroyo Group 2012:2).

Depot-Plaza Pedestrian Connection Study

The Pedestrian Study was prepared for the purpose of “evaluating the potential development of pedestrian corridors to encourage connections between the historic Plaza and surrounding business district to the Metrolink and OCTA Transit Center at the Orange Depot” (RRM Design Group 2008:1). The project area is within the boundaries of the study, which includes the area bounded by Maple Avenue, Glassell Street, Almond Avenue, and the railroad tracks. It is also in the “North Alley” pedestrian route area, which is between Chapman and Maple Avenues. The north alley is actually an “existing shortcut north of Chapman Avenue that runs east-west through spaces between buildings, alleys, and parking areas” (RRM Design Group 2008:16). Although pedestrians can cut through an existing alley north of Chapman Avenue between Lemon Street and Cypress Street, when they reach the project area, they have to travel either north to Maple Avenue or south to Chapman Avenue to connect to the depot (Ibid.). The Study recommends that redevelopment include “pedestrian-friendly paseos, plazas, and amenities” between Cypress Street and the depot (Ibid.). Several figures in the Pedestrian Study show that pedestrian connection through the subject project area.

The Pedestrian Study recommends that North Alley-area property owners be encouraged to “open a pedestrian pathway to complete the North Alley connection” and that outdoor dining and plaza areas be encouraged (RRM Design Group 2008:54).

Previous Documentation/Evaluations

401 West Chapman Avenue

The former car dealership building (30-159847, 401 West Chapman Avenue, APN 039-171-12) was documented in 1982, 1990, and 2005.

A Historic Resources Inventory form prepared by Heritage Orange County, Inc. in 1982, with an evaluation by AEGIS dated 1990, describes the building as a “brick commercial” style building dating to circa 1920 with no notable historical findings to date (AEGIS 1991:1 and 3). No information is provided regarding its potential historic significance.

In 2005, the building was documented on Department of Parks and Recreation (DPR) 523 forms as part of a reconnaissance survey. It was assigned a California Historical Resources (CHR) status code of 1D, indicating it is a contributor to a National Register-listed historic district (Chattel Architecture 2005a). The form describes it as a commercial building that is significant for its architecture, but assigned it the resource attribute code for an industrial building and listed “Industrial” as its architectural style (Ibid.). The period of significance is given as “Old Towne: Early Settlement (c. 1870–1920)” (Chattel 2005a:2). There is no formal evaluation using State, federal, or local criteria.

Also in 2005, the two additions to the dealership building were documented on separate DPR forms, possibly because, at that time, they had separate addresses: 112 North Cypress Street (north addition) and 403 West Chapman Avenue (west addition). The addition attached to the north elevation (112 North Cypress Street) was identified as a masonry building built circa 1920, but very little additional information was provided and it was not formally evaluated (Chattel Architecture 2005b). Despite the lack of information or justification, the addition was identified as a District contributor and assigned a CHR status code of 1D (Ibid.). The addition to the west elevation (403 West Chapman Avenue) was described as a small industrial building with no estimated date of construction and its architectural style is listed as Contemporary (Chattel Architecture 2005c). It was assigned a CHR status code of 6Z, indicating that it has no historical significance (Ibid.).

Based on research completed for the current study (Figure 6), it is known that the estimated construction dates in the previous evaluations are incorrect and, consequently, the periods of significance for the building and the northern addition are also incorrect.

The 401 West Chapman Avenue building is listed in the California Office of Historic Preservation's Built Environment Resources Directory (BERD) with CHR status codes of 1D and 5D2, indicating it is a contributor to a historic district listed in the National Register, as well as a district that is eligible for local designation. Neither of the additions is listed in the BERD.

107 North Atchison Street

The free-standing building at 107 North Atchison Street (APN 039-171-12) was also documented by Chattel Architecture in 2005. No estimated date of construction is provided and its architectural style is listed as Industrial. It was assigned a CHR status code of 6Z, indicating it is not historically significant (Chattel Architecture 2005d). This address is not included in the BERD.

135 North Atchison Street

The former transformer house (135 North Atchison Street, APN 039-171-03) was previously documented in 1991 and 2005. According to the 1991 evaluation, the building was a "very nice Mission style building at 109 N. Atchison [that] has been hidden from the street by an addition" (AEGIS 1991:1). It also states that the building is a good example of the Mission style and "would be a contributor to a Historic District and should be considered for nomination as a Local Landmark" (AEGIS 1991:2). The 1991 form references a 1982 survey of the building, but no survey form from that year was found. The 2005 evaluation reiterated much of what was included in the 1991 survey, but recommended that the building be a contributor to the Old Towne Orange Historic District and gave it the corresponding CHR status code of 3D (appears eligible for the National Register as a contributor to a National Register-eligible district through survey evaluation) (Chattel Architecture 2005e). A 2012 amendment to the 2005 DPR form that is based on the 2010 Historic Resources Inventory Update recommended that the status code be changed to 1D. The BERD does not list this address.

As part of this assessment, the DPR forms for 401 West Chapman Avenue (including its additions), 107 North Atchison Street, and 135 North Atchison Street have been updated and are included in Appendix A.

ARCHAEOLOGICAL FIELD SURVEY

The field survey of the project area revealed that the ground surface is completely obscured by commercial development and associated concrete and asphalt paving. However, the likelihood of subsurface archaeological residue is moderate based on the 1910s to 1920s era of construction.

ARCHITECTURAL SURVEY

401 West Chapman Avenue

This roughly L-shaped building is located in the southern part of the project area and includes the former auto dealership and three additions (Figures 11 through 22). It is currently occupied by a retail store.

The original One-Part Commercial Block style dealership building is rectangular in plan and constructed of brick with soldier courses. It is surmounted by a low-pitched barrel roof with parapets and clerestory windows. The south-facing façade retains a high degree of integrity and features four decorative finials evenly spaced atop the parapet, a stringcourse, four wall-mounted medallions, and large, recessed storefront windows with transoms on either side of the main entrance (Figure 11). The recessed main entrance features three transom windows above a pair of wood-framed glass doors flanked by large sidelights. There are very narrow flat canopies above the windows and entrance. This elevation retains a high level of historic architectural integrity.

The east elevation, adjacent to Cypress Street, has two decorative finials atop the stepped parapet, a stringcourse, two wall-mounted medallions, and a group of three large storefront windows with transoms above a narrow, flat canopy (Figures 12 through 14). The southern section of the east elevation retains a high degree of integrity, but the remainder of the east elevation has been compromised by alterations, including an addition (discussed below). It includes an unadorned brick wall with a recessed modern pedestrian door below a wide lintel and two metal bay/overhead doors flanking two, large metal-framed, multi-paned windows all below a single, long lintel. The brick wall appears to have three brick patches where three large window openings have been filled in. The north end of the east elevation is an addition that makes up the north (rear) elevation of the building.

The north (rear) elevation has a stepped parapet visible above an addition (Figure 15). The wood-framed addition (1938–1947) is rectangular in plan and extends the width of the original building. It has a shed and flat roof sheathed in composition sheets and a parapet on the east end. The east elevation of the addition (adjacent to Cypress Street) is covered with plaster and has a bay door and a wood-shingled pent roof above a modern pedestrian door. The north elevation has bay doors and is internally divided into at least two spaces. The west side of the addition has one pedestrian door and the north and west elevations are clad in non-original manufactured wood siding.

The west elevation of the original building is unadorned, painted brick and has one large bay door that has been partitioned to include a pedestrian door. A large L-shaped addition (described below) and the partitioned door have compromised the integrity of this elevation.

As previously noted, there are two additions that form an L-shape on the west side of the original building (Figures 19 through 22). The older and larger addition (1938–1947) extends west to Atchison

Street and is addressed as 403 West Chapman Avenue. It is wood-framed, rectangular in plan, and surmounted by a low-pitched, gable roof with a parapet and sheathed with composition sheets. The north elevation of this addition has non-original manufactured wood siding, a pedestrian door, and two large garage doors. The west elevation of this addition has a stepped parapet and is covered with stucco. The south elevation, which is also covered with stucco, has modern aluminum-framed sliding windows and modern French doors. This addition connects to a newer (1947–1950) and smaller wood-framed addition that is open on the south side and connects to the original building.

All of the additions to the original dealership building are in fair to good condition, but all have sustained alterations. They are utilitarian structures that do not exhibit any particular architectural style or character.



Figure 11: 401 West Chapman Avenue, façade/south elevation. View to the north (11/20/18).



Figure 12: 401 West Chapman Avenue, façade and east elevation. View to the northwest (11/20/18).



Figure 13: 401 West Chapman Avenue, east elevation detail. View to the west showing filled in windows (11/20/18).



Figure 14: 401 West Chapman Avenue, east elevation, including circa 1940 addition. View to the southwest (11/20/18).



Figure 15: 401 West Chapman Avenue, rear (north) elevation. View to the south showing the circa 1940 addition (11/20/18).



Figure 16: 401 West Chapman Avenue, west elevation and partial north elevation. View to the east (11/20/18).



Figure 17: 403 West Chapman Avenue, north elevation. View to the southwest (11/20/18).



Figure 18: 403 West Chapman Avenue, north and west elevations. View to the southeast (11/20/18).



Figure 19: 403 West Chapman Avenue, south elevation. View to the northwest showing alterations (windows and doors) to this addition (10/31/17).



Figure 20: 401–403 West Chapman Avenue, west and south elevations. View to the east (11/20/18).



Figure 21: 401 West Chapman Avenue, portion of west elevation. View to the southeast (11/20/18).



Figure 22: 401-403 West Chapman Avenue, overview of west and south elevations. View to the northeast (11/20/18).

107 North Atchison Street

This vernacular free-standing garage is adjacent to North Atchison Street (Figures 23 and 24). It is rectangular in plan and surmounted by a catslide gable roof sheathed with corrugated metal. It has narrow eaves and the exterior walls are clad with non-original manufactured wood siding. Aside from small attic vents, there are no openings on the east or west sides. The south elevation has two large garage doors. The north elevation was obscured from view. The building is in good condition, but the non-original wood siding has compromised its integrity.



Figure 23: 107 North Atchison Street, west elevation. View to the east (11/20/18).



Figure 24: 107 North Atchison Street, south and east elevations. View to the northwest (11/20/18).

135 North Atchison Street

This building is located in the northern part of the project area adjacent to North Atchison Street. It is a small Mission Revival style building with a corrugated metal addition (Figures 25 through 28). The one-story building is rectangular in plan and surmounted by a low-pitched, gable roof sheathed with corrugated metal and features Mission Revival style parapets on the east and west ends. The visible

exterior walls are covered with stucco. The east elevation has a wood-framed, four-paned double-hung window and a wood and glass door, both of which appear to be original. The door and the window are each recessed in segmental arched openings accented with concrete “keystones.” The door also has wide concrete trim. The north and south elevations of the Mission Revival style building have windows identical to the one in the east elevation (the north one is visible from inside the metal addition). The visible parts of the building are in good condition and retain a high degree of integrity.

The L-shaped corrugated metal addition wraps around the west and north sides of the Mission Revival style building completely obscuring these elevations of the original building and compromising the building’s integrity. The addition appears to consist of two buildings, both with gable roofs with parapets sheathed with corrugated metal. There are openings on the south, west, and east sides. The addition is in poor condition and, judging from the patchwork of metal panels, has sustained various alterations. Based on what is visible within the addition and through the windows, it is possible that, if revealed, the original elevations may retain at least a moderate degree of integrity.



Figure 25: 135 North Atchison Street, façade. View to the east (11/20/18).

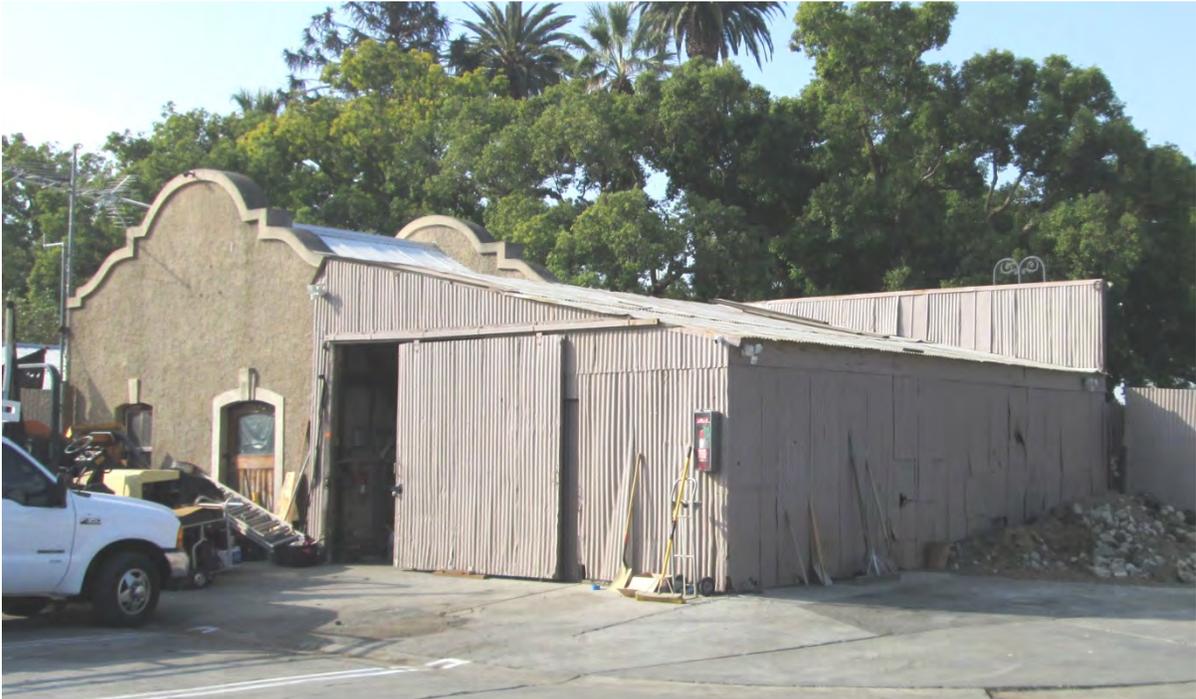


Figure 26: 135 North Atchison Street, east and north elevations. View to the southwest (11/20/18).



Figure 27: 135 North Atchison Street, door in east elevation (11/20/18).



Figure 28: 135 North Atchison Street, window in north elevation (11/20/18).

SIGNIFICANCE EVALUATION

Based on the research and field survey results discussed above, the following sections present the historical significance evaluations for the historic-period buildings in the project area. Pursuant to the City's requirements, the buildings are being evaluated under the criteria for the National Register of Historic Places (National Register) and the California Register of Historical Resources (California Register).

DEFINITIONS

CEQA (PRC Chapter 2.6, Section 21083.2 and CCR Title 145, Chapter 3, Article 5, Section 15064.5) calls for consideration of archaeological and historical resources in the environmental review process. The criteria for determining the significance of prehistoric and historic resources and potential project impacts to them are based on Section 15064.5 of the *CEQA Guidelines* and *Guidelines for the Nomination of Properties to the California Register*. Properties eligible for listing in the California Register and subject to review under CEQA are those meeting the criteria for listing in the California Register, National Register, or designation under a local ordinance.

National Register of Historic Places (36 CFR Part 60.4)

The National Register criteria for evaluation exclude properties that are less than 50 years old unless they are of exceptional importance. "Fifty years is a general estimate of the time needed to develop historical perspective and to evaluate significance. This consideration guards against the listing of properties of passing contemporary interest and ensures that the National Register is a list of truly historic places" (National Park Service 1995:41). In addition to meeting at least one of the National Register criteria, "historic properties must retain integrity. Within the concept of integrity, the National Register criteria recognizes seven aspects or qualities that, in various combinations, define integrity" (National Park Service 1995:44). These are: location, design, setting, materials, workmanship, feeling, and association (discussed in more detail below). "To retain integrity a property will always possess several, and usually most, of the aspects" (Ibid.). Guidance for applying the National Register criteria is provided in National Register Bulletin 15, How to Apply the National Register Criteria for Evaluation.

Pursuant to 36 CFR Part 60.4: The quality of significance in American history, architecture, archeology, engineering, and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association and

- A. That are associated with events that have made a significant contribution to the broad patterns of our history; or
- B. That are associated with the lives of persons significant in our past; or
- C. That embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
- D. That have yielded, or may be likely to yield, information important in prehistory or history.

“Ordinarily cemeteries, birthplaces, or graves of historical figures, properties owned by religious institutions or used for religious purposes, structures that have been moved from their original locations, reconstructed historic buildings, properties primarily commemorative in nature, and properties that have achieved significance within the past 50 years shall not be considered eligible for the National Register. However, such properties will qualify if they are integral parts of districts that do meet the criteria or if they” meet certain criteria considerations (National Park Service 1995:2). None of the resources associated with this project fall into these categories, therefore, these criteria considerations are not discussed further.

Integrity

Integrity is the ability of a property to convey its significance. The evaluation of integrity is sometimes a subjective judgment, but it must always be grounded in an understanding of a property’s physical features and how they relate to its significance. Historic properties either retain integrity (this is, convey their significance) or they do not. To retain historic integrity, a property will always possess several, and usually most, of the aspects. The retention of specific aspects of integrity is paramount for a property to convey its significance. Determining which of these aspects is most important to a particular property requires knowing why, where, and when the property is significant. National Register Bulletin 15 elaborates on the seven aspects of integrity as follows:

- a) *Location* is the place where the historic property was constructed or the place where the historic event occurred.
- b) *Design* is the combination of elements that create the form, plan, space, structure, and style of a property.
- c) *Setting* is the physical environment of a historic property.
- d) *Materials* are the physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property.
- e) *Workmanship* is the physical evidence of the crafts of a particular culture or people during any given period in history or prehistory.
- f) *Feeling* is a property’s expression of the aesthetic or historic sense of a particular period of time.
- g) *Association* is the direct link between an important historic event or person and a historic property (National Park Service 1995:44 and 45).

California Register of Historical Resources (14 CCR 15064.5)

The California Register criteria are based on National Register criteria and also typically require that a resource be 50 years of age or older in order to be considered for historical significance. The integrity of the resource, using the seven aspects of integrity discussed above, must also be taken into consideration. “It is possible that historical resources may not retain sufficient integrity to meet the criteria for listing in the National Register, but they may still be eligible for listing in the California Register” (California Office of Historic Preservation 1999).

Any object, building, structure, site, area, place, record, or manuscript which a lead agency determines to be historically significant or significant in the architectural, engineering, scientific, economic,

agricultural, educational, social, political, military, or cultural annals of California may be considered to be an historical resource, provided the lead agency's determination is supported by substantial evidence in light of the whole record. Generally, a resource shall be considered by the lead agency to be "historically significant" if the resource meets the criteria for listing on the California Register of Historical Resources (Pub. Res. Code, 5024.1, Title 14 CCR, Section 14 CCR, Section 4852) including the following:

1. Is associated with the events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
2. Is associated with the lives of persons important in our past;
3. Embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values; or
4. Has yielded, or may be likely to yield, information important in prehistory or history.

City of Orange

The City of Orange only has criteria for evaluating historic districts. Since the property is within the locally designated and National Register-listed Old Towne Orange Historic District, there is no need to consider the potential for a historic district. However, the following evaluations do address whether the buildings qualify as contributors to that District.

EVALUATION

In summary, the project area is developed with three historic-period buildings: a 1924 One-Part Commercial Block former car dealership with three additions built between 1938 and 1950 (401 West Chapman Avenue); a vernacular, freestanding garage built between 1950 and 1960 (107 North Atchison Street); and a circa 1912 Mission Revival style former transformer house with a large corrugated metal addition built between 1938 and 1947 (135 North Atchison Street). Research identified one person of note who was associated with the project area. Harry D. Riley, who was the first occupant of the dealership (1924 through 1927), is associated with the removal of the KKK from the Anaheim City Council in 1925. Mr. Riley lived in Anaheim and Santa Ana in the 1920s and 1930s and was very active in both of those communities. Research identified a few other extant former dealership buildings within a 20-block area around the subject property. These buildings appear to date to the 1920s and all have been altered to accommodate new uses. The buildings at 401 West Chapman Avenue and 135 North Atchison Street were previously documented as contributors to the National Register-listed Old Towne Orange Historic District, which has a period of significance of 1888 to 1940.

Because the National Register and California Register criteria are so similar, the criteria are addressed together to avoid redundancy.

National Register Criterion A: *Associated with events that have made a significant contribution to the broad patterns of our history.*

California Register Criterion 1: *Associated with the events that have made a significant contribution to the broad patterns of California's history and cultural heritage.*

According to National Register Bulletin 15, “to be considered for listing under Criterion A, a property must be associated with one or more events important in the defined historic context” (National Park Service n.d.:12). This may be a specific event or a pattern of events, but “mere association with historic events or trends is not enough, in and of itself ... the property’s specific association must be considered important as well” (National Park Service n.d.:12).

401 West Chapman Avenue. The former dealership building is associated with two important patterns of events: development of the downtown area as a commercial center and early 20th century automotive culture. However, no evidence was found indicating that it played an important role in either event. Development of the downtown area began several decades earlier and, by 1922, included at least 20 automotive-related businesses, including at least three dealerships. While construction of this dealership was part of the trends of downtown commercial development and automotive-related development in particular, it did not initiate either trend or change either trend in any important ways.

In terms of the evaluation criteria in the automotive dealership context, the building does not have a strong association with a particular dealership or automotive company. Research found that it has housed Studebaker, Chevrolet, and Ford dealerships, but no strong connection to any of these brands was found.

The building at 401 West Chapman Avenue is not individually eligible for listing in the National Register or California Register under Criteria A/1. However, the original part of the building (excluding all three additions) remains a contributor to the Old Towne Orange Historic District. It was built within the District’s period of significance (1888–1940) and, under this criterion, is associated with the District’s themes of commerce and transportation.

107 North Atchison Street. This post-World War II garage building is associated with automotive car culture in the second half of the 20th century. However, no evidence was found that it is associated with any significant contributions to that theme. Furthermore, its nondescript, temporally ambiguous appearance does not convey its association with the period when it was built. It is not individually eligible for listing in the National Register or California Register under Criteria A/1. It was built outside the period of significance (1888–1940) of the Old Towne Orange Historic District and does not contribute to the District’s significance.

135 North Atchison Street. This building was originally a transformer house and later an automotive repair shop. It is associated with the early industrial development of Orange and with early 20th century automotive culture. Because it has not functioned as a transformer house since at least the mid-1920s and all of the original equipment associated with its use as a transformer house has been removed, it does not convey its association with that function or early industrial development. As early as the mid-1920s, it was converted to an automotive repair shop and storage building. Sometime between 1938 and 1947, a large corrugated metal addition was added to the north and west elevations. The addition has obscured the original building and given it a nondescript, utilitarian appearance that does not convey an association with any particular function or period. Therefore, this building is not individually eligible for listing in the National Register or California Register under Criteria A/1 and does not contribute to the Old Towne Orange Historic District under these criteria.

National Register Criterion B: *Associated with the lives of persons significant in our past.*

California Register Criterion 2: *Associated with the lives of persons important in our past.*

According to National Register Bulletin 15, “persons associated with the property must be *individually* significant within a historic context” (National Parks Service n.d.:15). “Properties eligible under Criterion B are usually those associated with a person’s *productive life*, reflecting the time period when he or she achieved significance” (Ibid.).

401 West Chapman Avenue. This building was occupied by Harry D. Riley’s Studebaker dealership from 1924 to 1928. Mr. Riley moved to Anaheim in 1919 and was active in civic matters there until he relocated to Santa Ana in 1929. In 1925, as leader of the Anaheim Chamber of Commerce, Riley helped successfully remove several members of the KKK from the Anaheim City Council and other government positions. Mr. Riley’s role in this important event in Anaheim’s history is not directly related to his dealership in Orange. His associations with Anaheim and later Santa Ana, are lengthier and stronger than his brief association with Orange.

In terms of the evaluation criteria in the automotive dealership context, there is no indication that Mr. Riley played a leadership role in the automotive sales business in Orange and he only leased the building at 401 West Chapman Avenue for three years before relocating to Santa Ana. He has longer associations with other cities and properties.

The building at 401 West Chapman Avenue is not individually eligible for listing in the National Register or California Register under these criteria, nor does it qualify as a contributor to the Old Towne Orange Historic District under Criteria B/2.

107 North Atchison Street. No information about the owners/occupants during the historic period was found. This building is not individually eligible for listing in the National Register or California Register under these criteria, nor does it qualify as a contributor to the Old Towne Orange Historic District under Criteria B/2.

135 North Atchison Street. Very little information was found for owners/occupants of this building during the historic period and none appears to be historically important. This building is not individually eligible for listing in the National Register or California Register under these criteria, nor does it qualify as a contributor to the Old Towne Orange Historic District under Criteria B/2.

National Register Criterion C: *Embodies the distinctive characteristics of a type, period, or method of construction, or that represents the work of a master, or that possesses high artistic values, or that represents a significant and distinguishable entity whose components may lack individual distinction.*

California Register Criterion 3: *Embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values.*

Criterion C has four components to consider: 1) distinctive characteristics of a type, period, or method of construction; 2) work of a master; 3) high artistic values; and 4) significant and distinguishable entity. Most often properties are eligible under the first component. This component requires the property to retain enough “distinctive characteristics” (i.e., physical features common to individual

types, period, or methods) to be considered a true representative of a particular type, period, or method of construction (National Park Service n.d.:18). A property is not eligible simply because it has been identified as the only such property, it must be demonstrated to be significant as well (Ibid.).

Properties that are eligible under the second component must be the work of a person of recognized greatness in a field (National Park Service n.d.:19). In addition, the property must express a particular phase in the development of the master's career, an aspect of his/her work, or a particular idea or theme in his/her craft (Ibid.). "A property is not eligible as the work of a master, however, simply because it was designed by a prominent architect" (Ibid.).

Under the third component, "a property is eligible for its high artistic values if it so fully articulates a particular concept of design that it expresses an aesthetic ideal" as long as it expresses those ideals more fully than other properties of its type (National Park Service n.d.:19).

The fourth component refers to historic districts (National Park Service n.d.:19). As previously discussed, the project area is within a National Register-listed historic district, therefore, this component does not need to be considered.

401 West Chapman Avenue. This building has not been identified as the work of a master and it does not possess high artistic values. The building retains characteristics of a 1920s One-Part Commercial block auto dealership, such as its brick construction and symmetrical façade with simple decorative elements (medallions, stringcourse, and finials) and large showroom windows, but it no longer has a rectangular plan, which is a key characteristic of the type. The façade retains a high degree of integrity, but there are additions to the east, west, and north elevations. In addition, in the east elevation, three large windows have been removed/filled in and a modern door has been installed and, in the west elevation, a bay/overhead door has been partitioned to include a pedestrian door. Unlike many One-Part Commercial Block buildings, which front on one street and only have one or two visible elevations, this building is adjacent to three streets and each elevation and all of its additions and alterations is visible. Therefore, the compromised integrity of design, materials, and workmanship on the secondary elevations makes a greater negative impact than they normally might. Due to this lack of integrity, the building is not a true representative of a One-Part Commercial block auto dealership.

In terms of the evaluation criteria in the automotive dealership context, it is not an excellent example of an architectural style or architect's work with few or no alterations.

The building at 401 West Chapman Avenue is not individually eligible for listing in the National Register or California Register under Criteria C/3. However, the original building (excluding all three additions) remains a contributor to the Old Towne Orange Historic District. It was constructed during the District's period of significance (1888–1940) and contributes to the wide range of property types within the District.

107 North Atchison Street. This vernacular garage is nondescript and temporally ambiguous. There is no indication that it is the work of a master, it does not possess high artistic values, and it is not a true representative of a type, period, or method of construction. It is not individually eligible for listing in the National Register or California Register under these criteria and does not contribute to the significance of the Old Towne Orange Historic District under Criteria C/3.

135 North Atchison Street. No information was found indicating this altered Mission Revival style building is the work of a master. The building has Mission Revival style parapets and segmental arched openings, but is modest in size, character, and design and does not possess high artistic values. While the south and east elevations retain a high degree of integrity, the north and west (street-facing façade) elevations are hidden behind a large, corrugated metal addition. It is unknown what the integrity level of the north and west elevations is behind the addition. Because of the large addition, the building is not a true representative of the Mission Revival style.

The building at 135 North Atchison Street is not individually eligible for listing in the National Register or California Register under Criteria C/3. However, largely due to the relative rarity of examples of the Mission Revival style in the Old Towne Orange Historic District, the original building (excluding the addition) remains a contributor to the Old Towne Orange Historic District. It was constructed during the District's period of significance (1888–1940) and contributes to the wide range of architectural styles within the District.

National Register Criterion D and California Register Criterion 4: *Has yielded, or may be likely to yield, information important in prehistory or history.*

“The most common type of property nominated under this Criterion is the archeological site (or district comprised of archeological sites). Buildings, objects, and structures (or districts comprised of these property types), however, can also be eligible for their information potential” (National Park Service n.d.:21). To qualify under this criterion the property must be a source of data that has significant bearing on a research design that addresses issues such as data gaps, alternatives to existing theories, or priority areas identified under State or federal agency management plans (Ibid.).

401 West Chapman Avenue, 107 North Atchison Street, and 135 North Atchison Street. Each of these buildings and related additions was constructed in the 20th century using common construction materials and methods. There is no indication that they have any potential to yield information important in prehistory or history. No archaeological resources were identified in the project area. None of the buildings is eligible for listing in the National Register or California Register under Criteria D/4 and none contributes to the Old Towne Orange Historic District under these criteria.

IMPACTS ASSESSMENT

As discussed in the previous section, the buildings (excluding all additions) at 401 West Chapman Avenue and 135 North Atchison Street are contributors to the National Register-listed Old Towne Orange Historic District. Therefore, they are historical resources under CEQA and the potential project impacts to them must be analyzed.

CEQA establishes that “a project that may cause a substantial adverse change in the significance of a historical resource is a project that may have a significant effect on the environment” (PRC §21084.1). “Substantial adverse change,” according to PRC §5020.1(q), “means demolition, destruction, relocation, or alteration such that the significance of a historical resource would be impaired.” In order to determine whether the proposed project will result in substantial adverse changes to the historical resources, the character-defining features (CDFs) of each resource must be identified. The project components must then be analyzed to determine their potential impacts to the CDFs and the historical significance of the resources (401 West Chapman Avenue, 135 North Atchison Street, and the District). The proposed project is analyzed below using the SOIS and the HPDS. In addition, the Specific Plan and Pedestrian Study objectives are also considered.

CHARACTER-DEFINING FEATURES

CDFs are the visual aspects and physical features that make up the appearance of the historic-period building and typically include the shape of the building, its materials, craftsmanship, decorative details, and other features that contribute to its character (Nelson 1988). The alteration or removal of CDFs could result in substantial adverse changes to the significance of the resource. To the extent feasible, each of the CDFs listed below should be preserved.

- **401 West Chapman Avenue:**
 - The barrel roof, stepped parapets, and clerestory windows;
 - The brick walls (excluding modern brick patches);
 - The parapet-mounted finials, wall-mounted medallions, and stringcourse all on the façade;
 - The storefront windows in the south and east elevations, including the transoms and narrow, flat canopies;
 - The primary entrance in the south elevation, including the wood and glass doors, any original hardware, sidelights, transoms, and narrow, flat canopy;
 - The two bay openings (excluding any non-original doors), the two metal-framed, multi-paned windows, the lintels, and the Los Angeles Iron Works bumpers all in the east elevation; and
 - The bay opening and sliding bay door in the west elevation.
 - Features that are not CDFs include all of the additions and the pedestrian door and brick patches in the east elevation.
- **135 North Atchison Street:**
 - The Mission Revival style parapets;

- The original openings including the segmental arches with concrete “keystones”;
- The wood-framed, four-paned, segmental arch windows and brick sills;
- The wood-and-glass door in the east elevation, including its concrete surround and “keystone,” as well as any original hardware; and
- Any extant original openings and/or materials in the west and north elevations.
- The corrugated metal addition is not a CDF.

PROJECT DESCRIPTION

The project proposes to rehabilitate the historic-period buildings (identified as Building 1 and Building 4 on the project plans) for reuse as restaurants and construct two one-story commercial buildings (Buildings 2 and 3 on the project plans) and a small accessory building (Building 5 on the project plans) (Appendix B, Project Plans). The project also includes landscaping, three trash enclosures, and an east/west paseo.

Building 1 (401 West Chapman Avenue) rehabilitation of this building will involve the following:

- Removal of all of the existing additions.
- Installation of a canvas awning above the pedestrian entrance in the east elevation.
- Replacement of the southerly bay door in the east elevation with a large multi-paned window. No details for this window have been provided, but there does not appear to be any change to the size or configuration of the opening itself.
- Installation of a new pedestrian door (required emergency exit) in a portion of the northern bay/overhead door in the east elevation.
- Construction of a 1,330-square foot, one-story, wood-frame and plaster addition with a flat roof and parapet on the west elevation 43 feet from West Chapman Avenue and 45 feet from North Atchison Street.
 - The addition will be several feet lower than the historic-period building and will have smooth plaster walls.
 - A projecting circular bar area is proposed at the southwest corner of the addition. The bar will have a stepped metal roof with a conical central form that mimics the shape of the decorative finials atop the Building 1 parapet. The conical feature, which is essentially a canopy above open air seating, has a height of 18 feet, which is taller than the rest of the addition, but significantly lower than Building 1.
 - Based on the color elevations, the addition will be an off-white color.
- A free-standing canvas (translucent fabric) shade element will overlap a portion of the parapet of the addition and cover the parking/delivery/overflow dining area. The slightly curved shade element has a maximum height of 13 feet, which is considerably lower than Building 1.

- Outdoor dining areas will be located at the southwest corner of the property. They will be enclosed by 3-foot high smooth plaster walls topped by 3-foot high tempered glass panels with metal caps. Metal exit gates are proposed on West Chapman Avenue and North Atchison Street.
- Steel pickets are proposed along the north elevation of the proposed addition to Building 1. The project architect has stated that the multi-colored pickets are intended to emphasize the offset between the addition and the original Building 1 and to be part of an art program featured within the project area.
- The brick walls will be cleaned and sealed. The project architect has indicated that the cleaning process will involve gently washing the brick with soapy water, rinsing it with clean water, allowing it to dry, and then sealing it with a clear sealer. The washing will be non-abrasive and the sealer will be transparent.

Building 2 is a proposed one-story, 22-foot high, 3,470-square foot commercial building with the following features:

- Rectangular plan with east/west orientation. Located adjacent to the sidewalk along North Cypress Street.
- Gable roof with parapet and equipment well that will screen roof-mounted mechanical equipment. Interior has natural wood ceiling and exposed steel trusses.
- Canted skylights on the north elevation above a flat membrane roof with a metal fascia.
- Shallow eyebrow roof with metal fascia across the east elevation. This connects to the wide flat roof that extends across the most of the north elevation.
- Brick exterior walls on the south and west elevations.
- The east elevation (facing North Cypress Street) is dominated by a large, metal-framed storefront window that references the shape of the gable roof. The exterior walls around the window are clad with brick, featuring a soldier course, and smooth plaster below the window.
- The exterior of the north elevation is predominantly brick, but includes smooth plaster around a series of glass doors that open onto an outdoor dining area.
- The primary entrance is located at the east end of the north elevation adjacent to North Cypress Street and a service entrance is located at the west end of the north elevation.
- Based on the colored elevations and information stated by the project architect, the brick on this building will be different from, but complement the original brick on Building 1. The color of the plaster accents is not specified but, based on the colored elevations, appears to be slightly lighter than the brick and the eyebrow roof and flat membrane roof appear to be black.

Building 3 is a proposed one-story, 22-foot high, 4,140-square foot commercial building with the following features:

- Rectangular plan with east/west orientation. Located adjacent to the sidewalk along North Cypress Street.

- Barrel roof with parapet and equipment well that will screen roof-mounted mechanical equipment. Interior has natural wood ceiling and exposed steel trusses.
- Canted skylights on the south elevation above a flat membrane roof with a metal fascia.
- Shallow eyebrow roof with metal fascia across the east elevation. This connects to the wide flat roof that extends across most of the south elevation.
- Brick exterior walls on the north and west elevations.
- The east elevation (facing North Cypress Street) is dominated by a large, metal-framed, storefront window that references the shape of the barrel roof. The exterior walls around the window are clad with brick, featuring a soldier course, and smooth plaster below the window.
- The exterior of the south elevation is predominantly brick, but includes smooth plaster around a series of glass doors that open onto an outdoor dining area.
- The primary entrance is located at the east end of the south elevation adjacent to North Cypress Street and a service entrance is located at the west end of the south elevation.
- Based on the colored elevations and information stated by the project architect, the brick on this building will be darker than what is proposed for Building 2 and will complement the original brick on Building 1. The color of the plaster accents is not specified but, based on the colored elevations, appears to be slightly lighter than the brick and the eyebrow roof and flat membrane roof appear to be black.

Building 4 (135 North Atchison Street) will be rehabilitated as follows:

- The addition will be removed.
- The existing, exposed sheet metal roof will be retained.
- All original features and openings (i.e. windows and doors) will be preserved. Deteriorated or damaged features will be repaired or replaced in-kind.
- The exterior walls, which currently have a textured plaster (stucco) finish, will be maintained, patched, and repainted.
- Based on information provided on the plans, this building will remain a medium gray color with lighter trim.

Building 5 is a proposed one-story, 11.5-foot high, 416-square foot building with the following features:

- Rectangular plan with east/west orientation. Located adjacent to North Atchison Street.
- Flat roof that matches the addition to Building 1.
- Sliding barn-style doors on the south elevation.
- Smooth plaster exterior walls. Based on the colored elevations, the exterior walls will be an off-white color.

Other project features include:

- An east/west paseo north of Buildings 1 and 5 and south of Buildings 2 and 4.
- A small plaza with trees adjacent to North Atchison Street between Buildings 4 and 5.
- A parking lot, trash enclosures, and one pole light in the northwest corner of the project area adjacent to North Atchison Street.
- Landscaping in the form of trees and shrubs throughout the project area.
- A landscaped outdoor dining area between Buildings 2 and 3 that is enclosed by smooth plaster walls topped by metal pickets and divided with glass partitions between evenly spaced trees.
- A water feature on the east side of Building 5, which the project architect has indicated will include a sculpture element.
- A water feature and wood benches adjacent to North Cypress Street east of the outdoor dining area between Buildings 2 and 3.
- Property line fencing will include a 6-foot-high block wall along the north property line, a 3-foot plaster wall topped by 3 feet of glass along portions of the south and west property lines, and 6-foot high steel pickets and a gate on the west property line. No fences/walls are proposed on the east property line.

For consistency with the proposed plans, the discussions below identify the historic-period buildings as Buildings 1 and 4 and the proposed new construction as Buildings 2, 3, and 5.

PROJECT ANALYSIS

The *Secretary of the Interior's Standards (SOIS) for the Treatment of Historic Properties* (36 CFR Part 68) and the related Guidelines for applying the Standards are typically used to analyze potential project impacts to historical resources. The SOIS "are regulatory only for projects receiving Historic Preservation Fund grant assistance and other federally assisted projects. Otherwise, these Guidelines are intended to provide general guidance for work on any historic building" (Grimmer 2017:reverse title page). Projects that comply with the SOIS are considered to be mitigated to a level that is less than significant. The SOIS are divided into four treatment categories: preservation, restoration, rehabilitation, and reconstruction. Pursuant to 36 CFR Part 68.2.b, "rehabilitation means the act or process of making possible an efficient compatible use for a property through repair, alterations and additions while preserving those portions or features that convey its historical, cultural or architectural values." Because the proposed project involves the rehabilitation of the two historically significant buildings for adaptive reuse, the Standards for Rehabilitation are applicable.

Standards for Rehabilitation (36 CFR Part 68.3.b)

1. *A property will be used as it was historically or be given a new use that requires minimal change to its distinctive materials, features, spaces and spatial relationships.*

Building 1 (401 West Chapman Avenue). This building has historically housed commercial businesses. The proposed project will rehabilitate the building for continued use as a commercial business. The CDFs will be preserved with only minimal changes. A canvas canopy will be attached

to the east elevation brick wall, one large, new window will be installed where there historically was a bay door (opening will remain the same), an emergency exit door will be installed in the existing overhead door at the north end of the east elevation, and an addition will be attached to the west elevation brick wall where there has historically been an addition. These minor changes will not change the use or impair the historic significance of the building or the District.

Building 4 (135 North Atchison Street). For most of its history, this building has been used for commercial purposes. The proposed project will rehabilitate the building for continued commercial use and will preserve the CDFs. This will not impair the historic significance of the building or the District.

- The historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces and spatial relationships that characterize a property will be avoided.*

Historically, the project area has been developed with a variety of buildings and the proposed new construction will continue this pattern of development without overwhelming the two historic-period buildings. Removal of distinctive materials or alteration of features and spaces that characterize the property are not proposed, but could occur when the additions to the two historic-period buildings are removed. To ensure compliance with this Standard, the project plans include the following note: *Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize the buildings shall be preserved and/or repaired/replaced in-kind.*

- Each property will be recognized as a physical record of its time, place and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties will not be undertaken.*

No elements that create a false sense of historical development, such as conjectural features or architectural elements from other buildings are proposed.

- Changes to a property that have acquired historic significance in their own right will be retained and preserved.*

None of the alterations/additions to Buildings 1 and 4 have gained significance in their own right. Removal of the additions will not diminish the ability of the buildings to contribute to the Historic District or impair the significance of the District.

- Distinctive materials, features, finishes and construction techniques or examples of craftsmanship that characterize a property will be preserved.*

The distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize Buildings 1 and 4 are not proposed to be removed or otherwise altered. However, removal of the additions from Buildings 1 and 4 and construction of the new addition to Building 1 could inadvertently result in damage to the buildings. To ensure that care is taken and any damage is appropriately addressed, the project plans include a note stating: *Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize the buildings shall be preserved and/or repaired/replaced in-kind.*

- Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in*

design, color, texture and, where possible, materials. Replacement of missing features will be substantiated by documentary and physical evidence.

No deteriorated historic features have been specifically identified; however, to ensure that these types of issues are addressed appropriately the project plans include the following note: *Any deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a character-defining feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.*

- 7. Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.*

No chemical treatments are proposed. However, the architect has stated that the brick will be cleaned by gently washing it with soapy water, rinsing it, allowing it to dry, and then sealing it. The washing will be non-abrasive and the sealer will be transparent. To ensure compliance with this Standard, a note on the plans states: *Chemical or physical treatments, if appropriate, shall be undertaken using the gentlest means possible. Treatments that cause damage to historic materials shall not be used. Heavily soiled brick will be gently cleaned using soapy water and natural bristle brushes and rinsed with clean water. After drying, the brick will be sealed with a transparent sealer that will protect it from graffiti, pollution, and weather.*

- 8. Archeological resources will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.*

There were no archaeological resources identified within the project area. However, the entire project area has been developed and completely paved and surfaced as part of various commercial uses since the 1910s to 1920s, so there is some potential for intact subsurface cultural resources. Furthermore, the project is in an area that was previously identified by the City as potentially sensitive for archaeological resources. Therefore, it is automatically subject to the City's regulatory process, which requires monitoring by a qualified archaeologist during demolition and earthmoving activities. In addition, a note on the project plans indicates that a qualified archaeologist will be retained prior to issuance of demolition and/or grading permits.

- 9. New additions, exterior alterations or related new construction will not destroy historic materials, features and spatial relationships that characterize the property. The new work will be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the historic integrity of the property and its environment.*

Building 1 (401 West Chapman Avenue). One new addition is proposed to the west elevation of this building. Proposed exterior alterations to this building include installation of a canvas awning over the entrance in the east elevation, replacement of one bay door in the east elevation with a large window (opening will not change), and installation of an emergency exit door in the existing bay door in the east elevation. Related new construction includes proposed Buildings 2, 3, and 5.

The proposed awning is a minor alteration that will be clearly modern and compatible with the historic-period building in size and scale. It will not destroy historic materials, features, or spatial relationships that characterize the building. However, it is conceivable that its installation could result in some damage to the brick. As stated above, the project plans include a note that clearly

indicates that any damage would be repaired in a historically appropriate manner. Installation of the awning will not diminish the significance of the building or impair its ability to contribute to the District.

Alterations are proposed to both bay doors in the east elevation. The bay doors themselves are not CDFs, but the openings are and both openings will be maintained in the same size and configuration. No details have been provided for the proposed replacement of the southern bay door with a window or for the installation of an emergency door in the northern bay door; therefore, neither of these alterations can be fully analyzed. If completed in an appropriate manner, these alterations would be considered minor and easily reversible. They would not diminish the significance of the building or impair its ability to contribute to the District.

To ensure that the alterations to the bay doors are completed in a manner that is compatible with the historic character of Building 1 and the District, it is recommended that they be subject to review and approval by City staff prior to issuance of building permits. It is further recommended that both openings have doors/windows that are similar in appearance, design, and materials in order to maintain the current symmetry and maintain the visual character of the bay doors.

The proposed addition to the west elevation is compatible with Building 1 in terms of materials (smooth plaster), features (conical bar element), size (lower and smaller), scale and proportion (rectangular with a flat roof), and massing (boxy). The proposed smooth plaster exterior is complementary to and compatible with the exposed brick of Building 1. The nearby former packing house at 426 West Almond Street and its neighbor at the southwest corner of West Almond Street and South Cypress Street are both good examples of this combination of materials. The conical bar element references the decorative finials on the parapet of Building 1, while its metal structure differentiates it as modern. The generally rectangular, boxy shape and flat roof of the addition are also compatible with the scale, proportion, and massing of Building 1; the smaller size and height of the addition ensure that it is subordinate. Since there has historically been an addition in the same general location, the proposed addition will not substantially alter the spatial relationships that have historically characterized the property. The addition will not impair the significance of the building or its ability to contribute to the District.

Buildings 2 and 3. The proposed new construction of Buildings 2 and 3 will not destroy historic materials, features, or spatial relationships that characterize the property. As previously stated, the property has historically been developed with multiple buildings and Buildings 2 and 3 represent a continuation of this development pattern. Proposed Buildings 2 and 3 are compatible with historic-period Buildings 1 and 4 and other buildings in the Historic District in the following ways:

- **Materials.** The proposed materials are brick, plaster, and glass. There are numerous historic-period brick buildings in the immediate vicinity and throughout the District. Plaster, used as an accent or as the dominant wall surface, is also fairly widespread in the District, as are large metal-framed, multi-paned windows. Examples of buildings that incorporate one of more of these materials include Building 1 in the project area, the California Wire Company building a block north, the building on the southwest corner of West Chapman Avenue and South Cypress Street (south of Building 1), the Chase Bank building on the Plaza, and the buildings on the south side of West Almond Avenue between the railroad tracks and South Cypress Street.

Buildings 2 and 3 also have shallow eyebrow roofs and flat membrane roofs with exposed metal fascia. These minor metal accents lend a slight industrial flavor to the buildings that is in keeping with the metal roof of Building 4 and the historic character of the immediate vicinity. Examples of nearby industrial buildings and features include the railroad, the buildings at 128 and 160 South Cypress Street, and the California Wire Company building. The industrial flavor is also consistent with the objectives of the Specific Plan (discussed below).

The proposed building materials are compatible with the historic character of the area and will not diminish the significance of the District or its contributors.

- **Features.** Large windows dominate the east elevations (facing North Cypress Street) of Buildings 2 and 3. The shape of each window references the roof form of its building: the top of the Building 2 window is angled like a front gable and the top of the Building 3 window is curved like a barrel roof. While both roof forms as well as large, multi-paned, street-facing windows are common throughout the District, the shapes of the windows proposed for Buildings 2 and 3 are not common within the District. This minor design element serves as a subtle reminder that the buildings, while compatible with the historic character of the area, are modern.

The canted, glass skylights reference the clerestory windows on Building 1. Clerestory windows were fairly common on early 20th century industrial, manufacturing, and commercial buildings and are also present on Historic District contributors (128 and 210 South Cypress Street and 426 West Almond Avenue) within a block or two of the project area. The skylights are an updated design of a feature found in the District. They are compatible with the historic character of the area and will not diminish the significance of the District or its contributors.

Buildings 2 and 3 also include several glass doors along the interior elevations that open onto an outdoor dining area. These doors, which are a distinctly modern feature, are set back from and perpendicular to the right-of-way. They open onto a landscaped area enclosed by a low wall and metal pickets and will only be minimally visible from the right-of-way and the rest of the project area. This feature, like the skylights and the shape of the street-facing windows, helps identify Buildings 2 and 3 as modern construction, but does not diminish the historic character of the area or the significance of the District or its contributors.

Finally, as previously noted, there is an outdoor dining area between Buildings 2 and 3. This area is approximately 30 feet wide and creates a view corridor to Building 4 from North Cypress Street. Both buildings are approximately 20 feet east of Building 4 and offset from it so that they do not distract from, overshadow, or diminish the significance of Building 4.

- **Size, Scale, Proportion, and Massing.** Buildings 2 and 3 have a boxy, rectangular plan and a zero setback that are similar to numerous historic-period buildings in the District, including Building 1. They smaller than Building 1 in terms of height and square footage and do not overshadow Building 1 in any way. The east/west orientation of Buildings 2 and 3 puts their narrowest elevations adjacent to the street giving them less frontage on North Cypress Street than Building 1 and further emphasizing their subordinate nature in relation to the historic-period building. In terms of size, scale, proportion, and massing, Buildings 2 and 3 are compatible with the historic character of the area and will not diminish the significance of the District or its contributors.

Building 4 (135 North Atchison Street). No additions to this building are proposed. Instead, the corrugated metal addition will be removed, which will reduce the size of the building and slightly change the spatial relationships within the property. However, this is a minor change that is more than offset by the prospect of revealing the Mission Revival style structure behind the addition. The exposed sheet metal roof will be retained, as will the textured plaster (stucco) exterior, which will be patched and repainted. As noted on the project plans, if any of the CDFs are damaged, they will be repaired in a historically appropriate manner. The architect has also stated that all of the extant original openings (doors and windows) will be preserved. Because only the south and east elevations were visible during the field survey, it is recommended that when the addition is removed, the openings be verified and, if necessary, the project plans be revised to reflect all of the openings accurately. Proposed changes to Building 4 will not impair its significance or its ability to contribute to the significance of the Historic District.

Building 5. The proposed new construction of Building 5 will not destroy historic materials, features, or spatial relationships that characterize the property. As previously stated, the property has historically been developed with multiple buildings and Building 5 represent a continuation of this development pattern. Proposed Building 5 is compatible with historic-period Buildings 1 and 4 and other buildings in the Historic District in the following ways:

- **Materials.** The proposed material is smooth plaster. There are numerous historic-period buildings in the immediate vicinity and throughout the District that utilize plaster either as an accent or the primary wall surface. Examples include the former depot and post office.
- **Features.** This small accessory building is utilitarian in character and does not incorporate any special architectural features.
- **Size, Scale, Proportion, and Massing.** This rectangular building will be 11.5 feet tall and 416 square feet and will have a flat roof. It is considerably smaller in size, scale, and massing than existing Building 1 and proposed Buildings 2 and 3. Although it is very slightly larger in area than Building 4 (minus the additions), due to the lack of architectural details, it has less scale and massing and will not overshadow Building 4 in any way. As an accessory building, its proportion to the other buildings is appropriate.

10. *New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.*

Building 1 (401 West Chapman Avenue). A new addition is proposed to be constructed on the west elevation of Building 1. As part of this construction, the original exposed brick wall and sliding bay door will be preserved. The essential form and integrity of the original building will be unimpaired if the addition is removed in the future. Similarly, removal of the proposed new, freestanding construction (shade element, walls/fences, Buildings 2, 3, and 5) will not impair the essential form and integrity of Building 1 or the significance of the District.

Building 4 (135 North Atchison Street). No additions to Building 4 are proposed. Future removal of all proposed new construction (addition to Building 1 and new Buildings 2, 3, and 5) would not impair the essential form and integrity of Building 4 or the significance of the District.

Historic Preservation Design Standards

The HPDS were written to be consistent with the SOIS, therefore, most projects that are consistent with one set of standards will be consistent with the other (City of Orange 2018:3). Based on the analysis under the SOIS above, the project is consistent with the SOIS. However, the HPDS do have specific standards for additions to historic-period buildings and infill construction, which City staff has indicated are worth addressing separately. Like the SOIS, the HPDS are intended to be flexible. Not all of the guidelines have to be met, but enough should be incorporated so that new construction is compatible with the District, while still clearly modern (City of Orange 2018).

The following paragraph relative to infill construction was taken from the HPDS pages 46 and 47.

Infill in historic districts may consist of constructing a new building on a vacant lot (primary building) or constructing additional buildings (secondary buildings) on a lot containing an existing building. Successful infill construction takes cues from the surrounding historic neighborhood and its buildings without creating an exact replica of a historic architectural style. New construction should be consistent with the mass, scale, materials, height, roof form, setbacks, and pattern of windows and doors of existing buildings on the street. The site design of an historic structure is an essential part of its character. The spacing and location of buildings on each lot within an historic neighborhood usually establishes a rhythm that is essential to the character of the neighborhood. The grouping of buildings, with uniform setbacks and street features, gives each neighborhood a strong sense of place. One of the first steps to designing an infill building is to look at other buildings on the block and determine what are the common design elements that create a consistent streetscape and neighborhood character. Contemporary interpretations of historic architectural styles are not discouraged, but the primary goal of infill construction should be to create a building that responds to its context within a historic neighborhood.

For purposes of identifying the common design elements that create a consistent streetscape and neighborhood character in the project vicinity, an area east of the railroad tracks that includes nine blocks where the Santa Fe Depot Specific Plan and Old Towne Historic District (District) overlap was considered. The boundaries of this area are the railroad tracks on the west, Olive Street on the east, West Maple Avenue on the north, and Palmyra Avenue on the south. Within these nine blocks, there are 116 properties (excluding parking lots) and, of those, 67 properties are District contributors. These properties include a mix of residential and non-residential buildings, often within the same block. Because of this mix of uses, the scale, massing, and setbacks of the buildings are not consistent. Residential properties, whether single-family homes or apartment buildings, typically have landscaped front yards, while most of the non-residential buildings are either sidewalk-adjacent or separated from the sidewalk by parking lots. The right-of-way configurations also vary, having sections with landscaped parkways, sections without parkways, and an inconsistent pattern of street trees. In this nine-block area, there are also at least four different styles of streetlights, as well as a variety of parking lot light styles. Cumulatively, the mixes of uses, setbacks, sidewalk/parkway treatments, and street lighting in this nine-block area present an inconsistent streetscape with an indistinct neighborhood character that lacks rhythm. Focusing just on the immediate project vicinity reveals a similar lack of cohesive character. Of the 15 properties adjacent to North Cypress between West Chapman Avenue and West Maple Avenue, including the project area, there are six District

contributors (three residential and three non-residential), seven non-contributors (all non-residential), two parking lots, two segments of landscaped parkway, six unevenly spaced street trees, and historic-period and modern buildings. Within these nine blocks, the unifying theme seems to be diversity.

1. *The location of new primary and secondary structures on a lot should be consistent with the historic pattern of front and side yard setbacks.*

Buildings 2 and 3. These buildings have zero setback on North Cypress Street. This is consistent with Building 1, buildings to the north on the west side of North Cypress Street, and many other non-residential historic-period buildings in the area.

Building 5. This small storage building is situated 2.5 feet behind the property line adjacent to North Atchison Street. This is consistent with existing development in the project area and most non-residential development in the surrounding area.

2. *New buildings should be similar in mass and scale to surrounding buildings.*

- a. *If a new building is larger than its neighbors, it should be modulated so that the appearance of the mass is located back from the street and is less visible.*

Buildings 2 and 3. These proposed buildings are shorter and smaller than Building 1 and are compatible with other nearby historic-period buildings in size, scale, and massing. The exception to this is the diminutive Building 4 (374 square feet), which is smaller than most of the other buildings in the District. Buildings 2 and 3 are proposed to be set back more than 20 feet from this building and will not overshadow or distract from this small building.

Building 5. This proposed building is comparable in size to Building 4, but is shorter and significantly smaller than most of the other nearby buildings in the District.

- b. *Properties with new construction are recommended to use the average Floor Area Ratio of historic properties on the surrounding street as a model for compatible new development.*

Buildings 2 and 3. These buildings, at 22 feet in height and 3,470 square feet and 4,140 square feet in area, respectively, are comparable to other nearby buildings in mass and scale.

Building 5. This building, at 15 feet in height and 416 square feet, is smaller in mass and scale than most other buildings in the area.

3. *The height and roof form of a new building should be compatible to surrounding historic buildings.*

- a. *Roofing materials and details should be similar to those found on historic properties.*

Buildings 2 and 3. These buildings are each 22 feet high, which is lower than Building 1 and compatible with the heights of other nearby buildings. The roofs of each building include a brick parapet, which is similar to Building 1 as well as many other historic-period, non-residential buildings in the District. Roof-mounted equipment will not be visible from the ground. The north elevation of Building 2 and the south elevation of Building 3 each have a series of canted skylights above a flat membrane roof with exposed metal fasciae. As previously discussed, both of these features reference elements on Building 1 and those found on other historic-period buildings in the District.

Building 5. This small building has a flat roof that is similar to many of the non-residential, historic-period buildings in the District.

- b. Dormers should be similar in size and style to historic properties.*

Not applicable. No dormers are proposed.

4. *A new primary building should have a main entrance and façade parallel to and facing the street.*

Buildings 2 and 3. These buildings both have large storefront windows in the east elevations that face North Cypress Street and entrances that are just a few steps from the sidewalk in the north (Building 2) and south (Building 3) elevations. Although neither building opens directly onto the North Cypress Street right-of-way, the street-facing elevations clearly read as the primary elevations, which is the intent of this standard.

Building 5. This building is not a primary building.

5. *The progression of public to private spaces from the street should be maintained.*

- a. A sheltered building entrance or front porch may be appropriate to create a transitional space from the street to the interior of the building.*

Buildings 2 and 3. The primary public entrance into each of these buildings maintains the progression of public to private spaces that is common for non-residential buildings throughout the District. Similar to many commercial buildings in the area, the entrances are a few feet back from the sidewalk and are sheltered, in this case by a wide, flat membrane roof.

Building 5. Not applicable. This is not a primary or public building.

6. *New construction should have a similar pattern of windows and doors on elevations visible from the street to those found in surrounding historic buildings.*

Buildings 2 and 3. The block as a whole does not have a consistent pattern of windows and doors since there is a mix of residential and non-residential buildings, as well as historic-period and modern buildings. In the larger nine-block area, some historic-period commercial buildings have a symmetrical pattern of façade doors and windows, such as Building 1 and 402–424 West Chapman Avenue. Others, including 201 and 324 W. Chapman Avenue, 220 Maple Avenue, and 412 West Almond Street, have an asymmetrical arrangement with the entrance located at or near one end of the façade. Most of the storefront windows have divided lights and are vertically-oriented. The pattern of windows and doors on the street-facing elevations of Buildings 2 and 3 is similar to and consistent with those found in the surrounding historic-period buildings. They are asymmetrical in their arrangements and have large, vertically-oriented storefront windows with divided lights.

Building 5. Not applicable. This is not a primary or public building.

7. *The use of traditional building materials found on historic buildings in the Historic District is encouraged for new construction.*

- a. Exterior materials shall be compatible with the size, scale, design, texture, reflectivity, durability and color of historic materials used on comparable historic buildings in the Historic District.*

Buildings 2 and 3. The compatibility of the materials/textures, size, scale, and designs of these buildings was discussed in detail under SOIS Standard 9 and, therefore, is not repeated here. Aside from glass, which is a traditional building material, no reflective materials are proposed as part of the new construction. Durability of the proposed materials is anticipated to meet or exceed the durability of the historic materials used throughout the District. Although exact colors have not been specified, based on the colored elevations and statements made by the project architect, the color of the brick on Building 2 will be different from, but complementary to, the original brick on Building 1. Similarly, the color of the brick on Building 3 will be darker than what is proposed for Building 2 and complement the original brick on Building 1. It is anticipated that the plaster accents on these buildings will be off-white, but that has not been specified. To ensure compatibility, it is recommended that City staff require approval of the colors for Buildings 2 and 3 prior to issuance of building permits.

Building 5. The compatibility of the material, size, scale, texture, and design of this building was discussed in detail under SOIS Standard 9 and, therefore, is not repeated here. No reflective materials are proposed for this building. Durability of the proposed materials is anticipated to meet or exceed the durability of the historic materials used throughout the District. Based on the colored elevations and information from the project architect, this building will be an off-white color that will be compatible with other buildings in the area. To ensure compatibility, City staff should approve the color of Building 5 prior to issuance of building permits.

- b. Use of simplified versions of traditional architectural details is encouraged.*

Buildings 2 and 3. The canted skylights on these buildings are a simplified version of the Building 1 clerestory windows and sawtooth roofs of other historic-period buildings in the District. The shallow eyebrow roof and flat membrane roofs reference similar forms found on Building 1 above the storefront windows and the horizontal line of the lintels in the east elevation. This horizontal element is also found in the form of awnings on a number of the commercial buildings in the District. Each building also includes a soldier course, which is found on many of the brick buildings in the District, including Building 1.

Building 5. Building 5 is utilitarian and has no decorative elements or details. It is typical of a secondary building of its type.

- c. Alternates to traditional building materials may be considered, if the alternate material is compatible with the design and appearance of comparable historic features on similar contributing buildings in the Historic District.*

Buildings 2 and 3. As previously stated, materials proposed for these buildings are brick, plaster, and glass, all of which are traditional building materials. In addition, the eyebrow roof and flat membrane roof will have metal fasciae. Metal is also a common material throughout the district (e.g., window frames, roofing material, bay doors, accents, and siding).

Building 5. Alternative materials are not proposed for this building.

- 8. The height, mass and scale of new secondary buildings should be minimized as much as possible.*

- a. *In general, secondary buildings should be no taller than the primary building. In limited areas, secondary buildings may be taller than primary buildings, if this condition is already typical of the streetscape of the surrounding blocks.*

Building 5. This secondary building is not taller than Buildings 1 or 4.

- b. *The design of secondary buildings should be subordinate to the primary building on the lot.*

Building 5. The design of Building 5 is utilitarian and devoid of decorative elements. It is subordinate to Building 1 in every way. Although it is slightly larger than Building 4, by virtue of its nondescript design devoid of any decorative features, it is clearly subordinate to it.

- c. *Historic accessory structures were typically utilitarian buildings with limited decorative elements. Basic rectangular building forms and simple roof configurations are appropriate.*

Building 5. Building 5 is a small, rectangular building with a flat roof. It is utilitarian in design with no decorative elements.

9. *Infill construction should adhere to the sections on Standards for Historic Residential Buildings – Setting or Standards for Historic Commercial Buildings – Setting.*

Standard 9 requires infill (i.e., new) construction to adhere to Standards for historic-period buildings in four key locations. Buildings 2 and 3, which front on North Cypress Street, and Building 5, which is adjacent to North Atchison Street, are not located in any of the four key areas where these Standards apply. In any case, the Standards (paraphrased below) were taken into consideration. To avoid redundancy, discussion is only provided where there are new issues or clarification is needed.

- a. *Historic sidewalks and streetlights should be preserved.* The sidewalks will remain, but will be repaired and patched as necessary. No changes to the streetlights are proposed.
- b. *Primary building entrances shall be oriented toward the street.*
- c. *Outdoor dining is encouraged in front of buildings in the Plaza District and Downtown Core.* Not applicable. The project area is not within these areas.
- d. *Front yard landscaping of residences converted to commercial uses should be retained.* Not applicable.
- e. *Exterior light fixtures shall be compatible with the architectural style of the building.* Proposed lighting includes tree up lights, string lights in trees, in-ground up lights, low wall and landscape lights, building wall lights, soffit lights, and one pole light. With the exception of the pole light, the proposed light fixtures are small and unobtrusive. The 20-foot tall pole light, consisting of a concrete base, steel pole, and square arm mount, is proposed adjacent to the northern property line in the trash enclosure area. The light will be located approximately 25 feet east of the North Atchison Street right-of-way and will be at least partially screened from view from the right-of-way by a tree that is proposed at the northwest corner of the property. This utilitarian and unobtrusive light is consistent with other parking lot lighting in the area. It will not detract from the historic character of the District.
- f. *Trash enclosures shall be integrated into the building design and on secondary elevations.* Trash enclosures are proposed in the parking area at the northwest corner of the property.

Although not integrated into the design of Buildings 2, 3, or 5, their location in the northwest corner of the property adjacent to the north property line is unobtrusive and far enough from the historic-period buildings that the trash enclosures do not distract from them in any way.

- g. New buildings and additions shall not exceed two stories, 30 feet, or the height of adjacent buildings, whichever is the lesser height.*

Building 1 addition. The proposed addition to Building 1 is one story, approximately 16 feet in height, and lower than Building 1. The addition appears to be approximately the same height as Building 4, but is located about 40 feet to the southeast and will not overshadow Building 4 in any way.

Buildings 2 and 3. Buildings 2 and 3 are both 22-foot tall, one-story buildings that are lower than Building 1. According to the project architect, Building 4 is approximately 22 feet tall, although the color elevations make it look a little shorter than Buildings 2 and 3. If there is a small difference in height, it is offset by the location and orientation of the buildings. Buildings 2 and 3 are oriented toward North Cypress Street and are separated by an approximately 30-foot wide outdoor dining area. Building 4, which is located between and approximately 20 feet west of Buildings 2 and 3, is oriented toward North Atchison Street. Building 4 is clearly visible from North Cypress Street and North Atchison Street and is framed by Buildings 2 and 3 rather than silhouetted against or hidden by them. For these reasons, the intent of this standard is met, if indeed Building 4 is slightly shorter than Buildings 2 and 3.

Building 5. This building is shorter than Building 1 and Building 4.

- h. In the Plaza District new construction with exposed sloped roofs is prohibited.* Not applicable. The project area is not in this district; however, Buildings 2, 3, and 5 comply with this standard.
- i. In the Plaza District a consistent building street wall with zero setbacks shall be maintained by all new construction.* Not applicable. The project area is not in this district; however, Buildings 2 and 3 comply with this standard and Building 5 is set back only a few feet from the sidewalk.
- j. In the Downtown Core and Spoke Streets, new construction shall be compatible with the prevailing pattern of setbacks on the same street.* Not applicable. Buildings 2, 3 and 5 are not on spoke streets; however, they do comply with this standard.
- k. For new construction, a 15-foot landscape area is required between all parking areas and any public right-of-way.* The proposed site plan shows a 10-foot landscaped setback adjacent to the parking area in the northwest corner of the property and an approximately 3-foot landscaped setback along the parking area south of Building 5. Both of these are less than what the standard specifies, but comparable with nearby conditions. For example, the landscaped setback provided between the Hilbert Museum parking lot and North Atchison Street just north of the project area appears to be 5 feet or less and there is almost no landscaping between the former depot's parking lot and North Atchison Street, west of the project area. The landscaped setbacks at the northwest corner of West Chapman Avenue and Lemon Street (east of the project area), as well as the southwest corner of West Chapman Avenue and the railroad tracks (southwest of the project area) also appear to be deficient. Although the proposed landscaped setbacks adjacent to the parking areas are less than what this standard specifies, they are comparable to or greater than those in the surrounding area and, therefore, compatible with the character of the District.

- l. For existing construction, landscape areas should be provided between all parking areas and any public right-of-way to the greatest extent feasible. Landscaping is proposed throughout the property, including between the parking lots and North Cypress Street.*

The HPDS for additions to historic-period buildings are paraphrased below. To avoid redundancy, discussion is only provided where there are new issues or clarification is needed.

- 1. Compatible additions should be smaller in mass, scale, and volume than the historic building.*
- 2. Additions should be located away from the street at the rear of the building.* The intent of this standard is to minimize the visibility of additions from the public right-of-way. However, Building 1 is adjacent to three streets and the rear of the building (north elevation) is not necessarily the farthest away from a street or the least visible location for an addition. The proposed addition is located on the west side of the building, which is the farthest location from all three streets. It is set back 43 feet from West Chapman Avenue, 45 feet from North Atchison Street, and 50 feet from North Cypress Street, which is on the other side of Building 1. This location complies with the intent of this standard.
- 3. After construction, it should be clear what is new and what dates to the historic-period.*
- 4. Traditional building materials shall be used, but may differ from the historic materials in size, scale, or profile.*
- 5. Roof forms should be compatible with the existing building.*
- 6. Window and door openings should be consistent with those in the historic building.* The west elevation of the Building 1 addition has two openings. The southern opening has a single, solid core door and the northern opening has a pair of solid core doors. All of these doors are similar to the existing man door in the east elevation of Building 1. The south elevation of the addition has one opening consisting of two double automatic glass doors (electric sliding pocket). These are clearly modern, but compatible with Building 1 in that they have a similar appearance to the large storefront windows in the south elevation of Building 1.
- 7. The addition should result in no more than minimal loss of historic materials. CDFs of the historic building should not be obscured, damaged, or destroyed.*

Santa Fe Depot Specific Plan

The stated purpose of the Specific Plan is to “build an environment around the Santa Fe Depot that supports and facilitates transit use by capitalizing on pedestrian traffic and encouraging a mix of employment, shopping and residential uses within easy walking distance of the Orange Transportation Center” (Arroyo Group 2012:2). To accomplish this, the Specific Plan calls for development that is “designed for pedestrians, with pedestrian linkages to the transportation center and connections to” other destinations within Old Towne Orange (Ibid.). The proposed project addresses this by providing a pedestrian-friendly, landscaped paseo through the middle of the project. This feature complements Depot Park across Atchison Street and facilitates pedestrian traffic between the depot and the heart of Old Towne, a few blocks to the east.

As discussed previously, one of the Specific Plan objectives is to maintain an industrial flavor in the vicinity of the depot. The proposed project achieves compliance with this objective through the use of brick as a primary building material and metal accents in the new construction (e.g., metal fascia and painted steel pipe screen).

Depot-Plaza Pedestrian Connection Study

The Depot-Plaza Pedestrian Connection Study recommends that North Alley-area property owners be encouraged to “open a pedestrian pathway to complete the North Alley connection” and that outdoor dining and plaza areas be encouraged (RRM Design Group 2008:54). It also calls for “pedestrian-friendly paseos, plazas, and amenities” between Cypress Street and the depot (RRM Design Group 2008:16). The proposed project is consistent with all of these objectives. Outdoor dining areas are proposed adjacent to Building 1 and between Buildings 2 and 3. In addition, the site plan provides a pedestrian-friendly plaza area adjacent to Building 4 and a clear east/west paseo between North Cypress Street and North Atchison Street, completing that segment of the North Alley pedestrian connection.

PROJECT ANALYSIS SUMMARY

The proposed project has been analyzed using the SOIS, HPDS, Specific Plan, and Pedestrian Study and has been found consistent with all of them. It will not result in any substantial adverse changes to the historical resources (401 West Chapman Avenue, 135 North Atchison Street, and the District).

For additional detail and clarification, LSA recommends that prior to issuance of building permits the City require approval of exterior colors for the Building 1 addition and Buildings 2, 3, 4, and 5; the alterations to the bay doors in the east elevation of Building 1; and verification of the Building 4 openings after removal of the addition.

RECOMMENDATIONS

The foregoing report has provided background information on the project area, outlined the methods used in the current study, and presented the results of the various avenues of research. As a result of this study, Buildings 1 and 4 (401 West Chapman Avenue and 135 North Atchison Street), excluding all additions, were evaluated as contributors to the National Register-listed Old Towne Orange Historic District. Buildings 1 and 4, as well as the District, are “historical resources” as defined by CEQA. Therefore, the proposed project was analyzed to determine whether it would result in any substantial adverse changes to the significance of these historical resources. The analysis included the *Secretary of the Interior’s Standards for the Treatment of Historic Properties* (SOIS) and the City’s Historic Preservation Design Standards (HPDS). The objectives and recommendations of the Santa Fe Depot Specific Plan and the Depot-Plaza Pedestrian Connection Study were also considered. As a result of these analyses, it was determined that the proposed project is in compliance with the SOIS and the HPDS and meets the objectives of the Specific Plan and Pedestrian Study. However, to provide additional detail and clarity, LSA recommends that the City consider applying the following conditions:

Prior to issuance of building permits:

1. The exterior colors for the Building 1 addition and Buildings 2, 3, 4, and 5 are subject to approval by City staff.
2. To ensure that the alterations to the bay doors are completed in a manner that is compatible with the historic character of Building 1 and the District, it is recommended that they be subject to review and approval by City staff. It is further recommended that both openings have doors/windows that are similar in appearance, design, and materials in order to maintain the current symmetry.
3. Because only the south and east elevations were visible during the field survey, it is recommended that when the Building 4 addition is removed, the openings be verified and, if necessary, the project plans be revised to reflect all of the openings accurately.

In addition, the following standard regulatory compliance measures regarding buried cultural resources are required in conformance with Section 15064.5(e) of the *CEQA Guidelines*, PRC Section 5097.98, and State Health and Safety Code Section 7050.5.

- If buried cultural materials are encountered during earthmoving operations associated with the project, all work in that area should be halted or diverted until a qualified archaeologist can evaluate the nature and significance of the finds.
- In the event human remains are encountered, State Health and Safety Code Section 7050.5 states that no further disturbance shall occur until the County Coroner has made a determination of origin and disposition pursuant to Public Resources Code Section 5097.98. The County Coroner must be notified of the find immediately. If the remains are determined to be Native American, the County Coroner will notify the Native American Heritage Commission (NAHC), which will determine and notify a Most Likely Descendant (MLD). With the permission of the landowner or their authorized representative, the MLD may inspect the site of the discovery. The MLD shall

complete the inspection within 48 hours of notification by the NAHC. The MLD will have the opportunity to offer recommendations for the disposition of the remains.

REFERENCES

AEGIS

- 1991 City of Orange Historic Resources Inventory, 109 North Atchison. Provided by the City of Orange.

Ancestry.com

- Var. A variety of records were accessed online in November 2017 and April and May 2018 at: <http://home.ancestry.com/>. These include city directories, voter registration records, and United States Census Data.

Arroyo Group, The

- 2012 Orange Santa Fe Depot Specific Plan. Accessed online in October 2020 at the City's website: <https://www.cityoforange.org/DocumentCenter/View/614/Santa-Fe-Depot-Specific-Plan-Update---Final-PDF>.

Bean, Lowell John, and Charles R. Smith

- 1978 Gabrielino. In *California*, edited by R.F. Heizer, pp. 538–549. *Handbook of North American Indians*, vol. 8, W.C. Sturtevant, general editor, Smithsonian Institution, Washington, D.C.

Bean, Lowell John, and Florence C. Shipek

- 1978 Luiseño. In *California*, edited by R.F. Heizer, pp. 550–563. *Handbook of North American Indians*, vol. 8, W.C. Sturtevant, general editor, Smithsonian Institution, Washington, D.C.

Beck, Warren A., and Ynez D. Haase

- 1974 *Historical Atlas of California*. Oklahoma City: University of Oklahoma Press.

Blackburn, Thomas C.

- 1962–1963 Ethnohistoric Descriptions of Gabrielino Material Culture. *Annual Reports of the University of California Archaeological Survey* 5: 1–50.

Brigandi, Phil

- 2011 City History – Old Towne. Originally accessed online via the Orange Public Library website in 2011. Accessed online in May 2018 in a slightly different format at: <http://www.cityoforange.org/938/Introduction>.
- 2017 Email correspondence with Mr. Brigandi, noted local historian, regarding the history of the project area and Harry D. Riley.

California Geological Survey

- 2002 *California Geomorphic Provinces*. California Geologic Survey Note 36. California Department of Conservation.

California Office of Historic Preservation

- 1999 California Register and National Register: A Comparison (for purposes of determining eligibility for the California Register), OHP Technical Assistance Series #6. Available online

at: <https://ohp.parks.ca.gov/pages/1069/files/technical%20assistance%20bulletin%206%202011%20update.pdf>.

2020 Built Environment Resource Directory. Obtained from the State's website in April 2020 at: https://ohp.parks.ca.gov/?page_id=30338.

Chapman University

2019 About Chapman. Our Story: Where We've Been, Where We're Going. Accessed online in January 2019 at: <https://www.chapman.edu/about/our-story/index.aspx>.

Chattel Architecture

2005a Department of Parks and Recreation form for Chapman W. 401, APN 039-171-12. Provided by the City of Orange.

2005b Department of Parks and Recreation form for Cypress N. 112, APN 039-171-12. Provided by the City of Orange.

2005c Department of Parks and Recreation form for Chapman W. 403, APN 039-171-12. Provided by the City of Orange.

2005d Department of Parks and Recreation form for Atchison N. 107, APN 039-171-12. Provided by the City of Orange.

2005e Department of Parks and Recreation form for Atchison N. 135, APN 039-171-03. Provided by the City of Orange.

City of Orange

n.d. Old Towne District. Accessed online in November 2018 at: <https://www.cityoforange.org/262/Old-Towne-District>.

2018 Historic Preservation Design Standards. Accessed online at the City's website in 2021: <https://www.cityoforange.org/DocumentCenter/View/5928/Historic-Preservation-Design-Standards-for-Old-Towne?bidId=>.

Var. Building permits for 401 West Chapman Avenue. On file at the City of Orange, Building Division.

Grimmer, Anne E.

2017 *The Secretary of the Interior's Standards for the Treatment of Historic Properties with Guidelines for Preserving, Rehabilitating, Restoring, & Reconstructing Historic Buildings*. Published by the United States Department of the Interior, National Park Service, Technical Preservation Services, Washington, D.C. Accessed online in February 2021 at <https://www.nps.gov/tps/standards/treatment-guidelines-2017.pdf>.

Heizer, R.F. ed.

1968 The Indians of Los Angeles County. Hugo Reid's Letters of 1852. *Southwest Museum Papers* 21. Los Angeles, California.

1978 *Handbook of North American Indians*, vol. 8: California. W.C. Sturtevant, general editor, Smithsonian Institution, Washington, D.C.

Historic Resources Group

- 1996 National Register of Historic Places Multiple Property Documentation Form for Early Auto-Related Properties in Pasadena, California. Accessed online in November 2018 at: https://npgallery.nps.gov/NRHP/GetAsset/NRHP/64500055_text.

Hudson, D. Travis

- 1971 Proto-Gabrielino Patterns of Territorial Organization in Southern Coastal California. *Pacific Coast Archaeological Society Quarterly* 7(2):49–76.

Johnston, Bernice E.

- 1962 *California's Gabrielino Indians*. (Frederick Webb Hodge Anniversary Publication Fund 8) Los Angeles: Southwest Museum.

Kroeber, Alfred L.

- 1976 *Handbook of the Indians of California*. Dover Publications, New York. Originally published 1925, Bulletin No. 78, Bureau of American Ethnology, Smithsonian Institution, Washington, D.C.

LP3 Architecture, Inc.

- 2020 Old Towne Gateway, 401 West Chapman Avenue, Orange, California. Plans prepared in December 2020.

Los Angeles Times

- 1924a Orange is Building Hub. January 27, page 109.
1924b Building in Orange on Up Grade. March 8, page 13.
1925a Four Secret Prisoners in Anaheim Klan War. January 27, pages 1 and 2.
1925b Klan Scoured in Resolution. January 13, page 10.
1925c Anti-Klan Forces Lead in Election at Anaheim. February 4, pages 1 and 3.
1959 Advertisement. Put a Boat in Your Budget. July 26, page 209 (third column).
1966 Advertisement. May 29, page 309.
1969 Advertisement. November 22, page 94.

Margolies, John

- 1993 *Pump and Circumstance, Glory Days of the Gas Station*. Bulfinch Press, Little, Brown and Company, Boston.

Moratto, Michael J.

- 2004 *California Archaeology*. Orlando, Florida: Academic Press. Originally published 1984.

National Park Service

- 1995 How to Apply the National Register Criteria for Evaluation. United States Department of the Interior, National Park Service, Cultural Resources.

Nelson, Lee H., FAIA

- 1988 Preservation Brief 17, Architectural Character-Identifying the Visual Aspects of Historic Buildings as an Aid to Preserving their Character. Technical Preservation Series, National

Park Service. Accessed online in January 2016 at: <http://www.nps.gov/tps/how-to-preserve/briefs/17-architectural-character.htm>.

Norris, R.M., and R.W. Webb

1976 *Geology of California*, John Wiley and Sons, Inc., Santa Barbara.

Orange County Archives

2017 Email correspondence in November 2017 that resulted in the Archives providing various information including 1938 and 1947 aerial photographs of the project area.

Var. Email correspondence in March 2021 that resulted in the Archives providing city directory information regarding various addresses in and around the project area.

Orange Daily News

1912 Application for Permit. October 17, page 1.

Riverside Art Museum

n.d. The First of Its Kind: Riverside Auto Center, 1965. Accessed online in November 2018 at: <http://www.riversideartmuseum.org/exhibits/exhibit-archive/first-its-kind-riverside-auto-center-1965/>.

RRM Design Group

2008 City of Orange Depot-Plaza Pedestrian Connection Study. Accessed online in April 2021 at: <https://www.cityoforange.org/DocumentCenter/View/11761/Depot-Plaza-Pedestrian-Connection-Study-2008?bidId=>.

Sanborn Fire Insurance Maps

1909 Accessed online in November 2017 and 2018 via the Los Angeles Public Library's website at: <https://www.lapl.org/>.

1922 Accessed online in November 2017 and 2018 via the Los Angeles Public Library's website at: <https://www.lapl.org/>.

1950 Accessed online in November 2017 and 2018 via the Los Angeles Public Library's website at: <https://www.lapl.org/>.

Santa Ana Register

1919 Longworth Sells His Garage at Anaheim. August 9, page 9.

1920a A Greater Orange County Industry. August 21, page 7.

1920b Anaheim Board to Advertise Section. January 14, page 6.

1920c Anaheim Boost Clubs to Unite. June 15, page 6.

1923 Orange Firm to Put Up Building. October 11, page 17.

1927a Orange Citizens to Vote on Park Project June 21; Circular Law is Passed. May 21, page 17.

1927b Police News. July 5, page 11.

1928a Advertisement. August 4, page 19.

1928b No title. April 8, page 2.

1929a Year Seen as Banner Period for Building. January 3, page 7.

1929b You and Your Friends. July 22, page 11.

- 1929c Harry D. Riley Announces Concentration of Business and Civic Interest in S. A. August 4, page 13.
1930 H. Doty Buys Interest in Car Company. July 20, page 10.

Schoenherr, Allan A.

- 1992 *A Natural History of California*. University of California Press, Berkeley and Los Angeles.

SeeCalifornia.com

- n.d. Orange, CA History. Accessed online in April 2021 at: <http://www.seecalifornia.com/history/orange-historic-district.html>.

Sharp, Robert P.

- 1976 *Southern California (K/H Geology Field Guide Series)*. Kendall/Hunt Publishing, Dubuque.

Wallace, William J.

- 1955 A Suggested Chronology for Southern California Coastal Archaeology. *Southwestern Journal of Anthropology* 11(3):214–230.
1978 Post-Pleistocene Archaeology. In *California*, edited by R. Heizer, pp. 550–563. *Handbook of North American Indians*, Vol. 8. W.C. Sturtevant, general editor. Smithsonian Institution, Washington, D.C.

Warren, Claude N.

- 1968 Cultural Tradition and Ecological Adaptation on the Southern California Coast. *Eastern New Mexico University Contributions in Anthropology* 1(3). Portales.
1984 The Desert Region. In *California Archaeology*, by M. Moratto with contributions by D.A. Fredrickson, C. Raven, and C. N. Warren, pp. 339–430. Academic Press, Orlando, Florida.

Warren, Claude N., and Robert H. Crabtree

- 1986 Prehistory of the Southwestern Area. In W.L. D’Azevedo ed., *Handbook of the North American Indians*, Vol. 11, *Great Basin*, pp. 183–193. Washington D.C.: Smithsonian Institution.

APPENDIX A

DEPARTMENT OF PARKS AND RECREATION (DPR) 523 FORMS

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # 30-159847

HRI # _____

Trinomial _____

NRHP Status Code 1D

Other Listings _____

Review Code _____

Reviewer _____

Date _____

Page 1 of 6

Resource Name or #: 401 West Chapman Avenue

P1. Other Identifier: 401-403 West Chapman Avenue

***P2. Location:** Not for Publication Unrestricted ***a. County:** Orange and (P2b and P2c or P2d. Attach a Location Map as necessary.)

***b. USGS 7.5' Quad:** Orange, CA **Date:** 1981 T4S; R9W; S.B.B.M.

c. Address: 401 West Chapman Avenue

City: Orange

Zip: 92866

d. UTM: Zone: 11; _____mE/ _____mN (G.P.S.)

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate): APN 039-171-12; northwest corner of West Chapman Avenue and North Cypress Street

***P3a. Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

This roughly L-shaped building has three additions and was previously documented in 1982/1990 and 2005. The original One-Part Commercial Block building is rectangular in plan and constructed of brick. It is surmounted by a low-pitched barrel roof with parapets and clerestory windows. The south-facing façade retains a high degree of integrity and features four decorative urns evenly spaced atop the parapet, a stringcourse, four wall-mounted medallions, and large, recessed storefront windows with transoms on either side of the main entrance. The recessed main entrance features three transom windows above a pair of wood-framed glass doors flanked by large sidelights. There are very narrow flat canopies above the windows and entrance. This elevation retains a high level of historic architectural integrity.

The east elevation, adjacent to Cypress Street, has two decorative urns atop the stepped parapet, a stringcourse, two wall-mounted medallions, and a group of three large storefront windows with transoms above a narrow, flat canopy. The southern section of the east elevation retains a high degree of integrity, but the remainder of the east elevation has been compromised by alterations. It includes an unadorned brick wall with a recessed modern pedestrian door below a wide lintel and two metal bay doors flanking two large metal-framed, multi-paned windows all below a single, long lintel. The brick wall appears to have three brick patches where three large window openings have been filled in. *See Continuation Sheet*

***P3b. Resource Attributes:** (List attributes and codes) HP6 – 1-3 story commercial building

***P4. Resources Present:** Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photo or Drawing (Photo required for buildings, structures, and objects.)



See Continuation Sheet

P5b. Description of Photo: (View, date, accession #) Façade, view to the north (11/20/18)

***P6. Date Constructed/Age and Sources:** Historic Prehistoric Both
1924 (Los Angeles Times 1924)

***P7. Owner and Address:**
Leason F. Pomeroy, FAIA
LP3 Architecture, Inc.
158 North Glassell Street
Orange, California 92886

***P8. Recorded by:** (Name, affiliation, and address)
Casey Tibbet, M.A.
LSA Associates, Inc.
1500 Iowa Avenue, Suite 200
Riverside, California 92507

***P9. Date Recorded:**
November 2018

***P10. Survey Type:** (Describe) Intensive-level CEQA compliance

***P11. Report Citation:** (Cite survey report and other sources, or enter "none.") Cultural Resources Assessment for the Old Towne Gateway Project, Assessor's Parcel Numbers 039-171-12 and 039-171-03, City and County of Orange, California, 2021. Prepared by LSA Associates, Inc.

***Attachments:** NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record Photograph Record Other (List):

BUILDING, STRUCTURE, AND OBJECT RECORD

B1. Historic Name: _____

B2. Common Name: _____

B3. Original Use: Auto dealership **B4. Present Use:** Commercial

***B5. Architectural Style:** One-Part Commercial Block

***B6. Construction History:** (Construction date, alterations, and date of alterations)

No original building permits were found for this building. In order to determine the history of the property, a variety of sources was reviewed including, but not limited to, Sanborn Fire Insurance Maps, historic aerial photographs, newspapers, City Directories, and building permits.

1923 News articles announced that A.H. Pease of Pease, Kolberg & Company planned to build two buildings both on West Chapman Avenue near Cypress Street (*Santa Ana Register* 1923). The subject building was to be "single story with a high ceiling adding an air of distinction to the place. The structure, which will be forty-six by eighty-two feet, will contain tile floors with marble base, mezzanine floor, offices, rest rooms and an attractive showroom rivaling anything of its kind in the county" (Ibid.).

1924 News article indicated that Pease-Kolberg was "erecting a beautiful building, which will be the home of the Studebaker people. The building will have a frontage of forty feet on Chapman and eighty-six feet on Cypress Street. There will be an upstairs where the offices and a luxurious woman's rest room will be" (*Los Angeles Times* 1924).

1950 Sanborn maps show an auto sales and repair building with concrete floors, wood truss roof, and skylight, which is the subject building (401 and 405 West Chapman Avenue), along with several additions (Sanborn Fire Insurance Maps 1950). The additions include an auto repair building with iron-on-frame construction and a concrete floor (attached to the west elevation of the dealership building); a patio cover (attached to the west elevation of the dealership and the south elevation of the repair building); a very small store in the middle of the used auto sales lot at the corner of Chapman Avenue and Atchison Street (411 West Chapman Avenue); and an auto repair building (118 North Cypress Avenue) with iron-on-frame construction and a concrete floor (attached to the north end of the dealership building, constructed between 1938 and 1948).

1993 The earliest permits found for 401 West Chapman Avenue are for exterior alterations and seismic retrofit.

1999 Permit indicates that a sign was added and the existing brick façade was replaced with a similar product.

2009 Permit issued to "repair car damaged wall front corner of building facing Cypress. Like for like."

2011 Sign permit was issued.

***B7. Moved?** No Yes Unknown **Date:** _____ **Original Location:** _____

***B8. Related Features:** This building shares the property with a circa 1955 garage/storage building (107 N. Atchison Street).

B9a. Architect: Unknown **b. Builder:** Unknown

***B10. Significance: Themes:** Commerce; Transportation **Area:** City of Orange

Period of Significance: 1924 **Property Type:** Commercial **Applicable Criteria:** A and C

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

This 1924 One-Part Commercial Block building is not eligible for individual listing in the National Register of Historic Places (National Register) or the California Register of Historical Resources (California Register). However, it continues to qualify as a contributor to the National Register-listed Old Towne Orange Historic District. It is a historical resource for purposes of the California Environmental Quality Act (CEQA).

Historic Context. Refer to the related report (see P11 on page 1).

B11. Additional Resource Attributes: (List attributes and codes)

***B12. References:** *See Continuation Sheet*

B13. Remarks:

***B14. Evaluator:** Casey Tippet, M.A., LSA Associates, Inc., 1500 Iowa Avenue, Suite 200, Riverside, California 92507

***Date of Evaluation:** January 2019

(Sketch Map with north arrow required.)

Refer to Location Map

(This space reserved for official comments.)

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____
HRI # _____
Trinominal _____

Page 3 of 6 *Resource Name or #: (Assigned by recorder) 401 West Chapman Avenue

*Recorded by LSA Associates, Inc. *Date: January 2019 Continuation Update

***P3a. Description:** (continued from page 1)

The north (rear) elevation of the original building has a stepped parapet. The wood-framed addition (1938–1947) is rectangular in plan and extends the width of the original building. It has a shed and flat roof sheathed in composition sheets and a parapet on the east end. The east elevation of the addition (adjacent to Cypress Street) is covered with plaster and has a bay door and a wood-shingled pent roof above a modern pedestrian door. The north elevation has bay doors and is internally divided into at least two spaces. The west side of the addition has one pedestrian door and the north and west elevations are clad in non-original manufactured wood siding.

The west elevation of the original building is unadorned, painted brick and has one large bay door that has been partitioned to include a pedestrian door. A large L-shaped addition and the partitioned door have compromised the integrity of this elevation. There are two additions that form an L-shape on the west side of the original building. The older and larger addition (1938–1947) extends west to Atchison Street and is addressed as 403 West Chapman Avenue. It is wood-framed, rectangular in plan, and surmounted by a low-pitched, gable roof with a parapet and sheathed with composition sheets. The north elevation of this addition has non-original manufactured wood siding, a pedestrian door, and two large garage doors. The west elevation of this addition has a stepped parapet and is covered with stucco. The south elevation, which is also covered with stucco, has modern aluminum-framed sliding windows and modern French doors. This addition connects to a newer (1947–1950) and smaller wood-framed addition that is open on the south side and connects to the original building.

All of the additions to the original dealership building are in fair to good condition, but all have sustained alterations. They are utilitarian structures that do not exhibit any particular architectural style or character.

P5a. Photo or Drawing (continued from page 1). For additional photographs refer to the related report (see P11 above).



Façade and east elevation. View to the northwest (11/20/18).



East elevation, including circa 1940 addition. View to the southwest (11/20/18).

See Continuation Sheet

CONTINUATION SHEET

Primary # _____

HRI # _____

Trinomial _____

Page 4 of 6 *Resource Name or #: (Assigned by recorder) 401 West Chapman Avenue*Recorded by LSA Associates, Inc. *Date: January 2019 Continuation Update***B10. Significance:** (continued from page 2)

People/Businesses Associated with this Building. In 1919, Harry D. Riley of San Diego purchased the T.L. Longworth garage in Anaheim (*Santa Ana Register* 1919). Riley was going to be the northern Orange County agent for Studebaker and had been the Ford agent in San Diego for six and a half years (*Ibid.*). By 1920, Riley was a member of the Board of Trade, a city advisory committee, and was advocating for advertising the city of Anaheim (*Santa Ana Register* 1920a). Riley was named the first president of the Anaheim Chamber of Commerce and held the position from 1920–1925; it was during this time he led the fight against the Ku Klux Klan in Anaheim (Brigandi 2017; *Santa Ana Register* 1920b). Research indicates that the petition circulated to recall the councilmen with ties to the Klan was actually sponsored by the U.S.A. Club, but the Chamber of Commerce led by Harry D. Riley did join in the fight (*Los Angeles Times* 1925a, 1925b). In January 1925, just weeks before the recall vote, the Anaheim Chamber published a resolution stating in part “Whereas, the present situation has developed beyond any political issue into a community crisis involving the entire future of our community ... the facts are clear that the Ku Klux Klan has not only gained control of the political affairs of our city, but has become a menace alike to the peace and business prosperity of our city ... and it is not only impossible to get industries to locate here under its domination, but it has become a question of holding several of our present leading industries ... therefore, be it resolved by this board of directors that we and the entire Chamber of Commerce throw our influence and efforts into the campaign to rid the city of this Ku Klux Klan control ... signed by Harry D. Riley, president” and others (*Ibid.*). After the February 3, 1925 vote the councilmen were recalled and replaced with anti-Klan candidates, as were their Klan-friendly appointees (Brigandi 2017; *Los Angeles Times* 1925c). Although this did not end all Klan activity, it was a decisive blow to the organization in north Orange County (Brigandi 2017).

During the 1920s, Mr. Riley lived in Anaheim and had at least three dealerships in Orange County (Ancestry.com var.). In 1928, Riley built a new garage at 503 South Main Street in Santa Ana and, in summer 1929, it was announced that the Riley family had moved from Anaheim to Santa Ana (*Santa Ana Register* 1929a, 1929b). A subsequent article reported that Mr. Riley had plans to expand his Santa Ana facility and would be concentrating his businesses and civic interests in that city since the leases on his buildings in Anaheim and Orange had expired in January 1929 (*Santa Ana Register* 1929c).

After Riley’s departure at the end of 1928, the building at 401 West Chapman Avenue was occupied by various auto-related businesses until at least 1966 (Chattel 2005; *Los Angeles Times* 1966). A combination of city directory listings and news advertisements revealed the following historic-period occupants at 401 West Chapman Avenue: Schwab Chevrolet Co. (1928), Mansur Motor Co. (Ford dealer, 1937–1953), and Selman Chevrolet (early 1966) (Orange County Archives var.; *Los Angeles Times* 1966). In addition, previous documentation for the building lists Sackett-Doty Chevrolet and Doty Chevrolet as former occupants in the late 1920s and early 1930s (Chattel 2005). In 1930, Doty bought out Schwab, but the 1932 City Directory lists Doty Chevrolet at 402 West Chapman Avenue as does a *Los Angeles Times* advertisement from 1934 (Orange County Archives var.; *Santa Ana Register* 1930). This location was repeated in numerous advertisements in 1934 and one in 1936. Advertisements put Clift’s Marine Shop at 403 West Chapman Avenue (west addition to 401) in 1959 and Speed & Marine Associates at 401 West Chapman Avenue in 1966 through at least 1969 (*Los Angeles Times* 1959, 1966, and 1969).

Significance Evaluation. This building is being evaluated under the National Register and California Register criteria. There are no local criteria for individual resources.

Under Criteria A/1, the former dealership building is associated with two important patterns of events: development of the downtown area as a commercial center and early 20th century automotive culture. However, no evidence was found indicating that it played an important role in either event. Development of the downtown area began several decades earlier and by 1922 included at least 20 automotive-related businesses, including at least three dealerships. While construction of this dealership was part of the trends of downtown commercial development and automotive-related development in particular, it did not initiate either trend or change either trend in any important ways. The building does not have a strong association with a particular dealership or automotive company. Research found that it has housed Studebaker, Chevrolet, and Ford dealerships, but no strong connection to any of these brands was found.

This building is not individually eligible for listing in the National Register or California Register under Criteria A/1. However, the original part of the building (excluding all three additions) remains a contributor to the Old Towne Orange Historic District. It was built within the District’s period of significance (1888–1940) and, under this criterion, is associated with the District’s themes of commerce and transportation.

Under Criteria B/2, this building was occupied by Harry D. Riley’s Studebaker dealership from 1924 to 1928. Mr. Riley moved to Anaheim in 1919 and was active in civic matters there until he relocated to Santa Ana in 1929. In 1925, as leader of the Anaheim Chamber of Commerce, Riley helped successfully remove several members of the KKK from the Anaheim City Council and other government positions. Mr. Riley’s role in this important event in Anaheim’s history is not directly related to his dealership in Orange. His associations with Anaheim and later Santa Ana, are lengthier and stronger than his brief association with Orange. There is no indication that Mr. Riley played a leadership role in the automotive sales business in Orange and he only leased the building at 401 West Chapman Avenue for three years before relocating to Santa Ana. He has longer associations with other cities and properties. (see *Continuation Sheet*)

CONTINUATION SHEET

Primary # _____

HRI # _____

Trinomial _____

Page 5 of 6 *Resource Name or #: (Assigned by recorder) 401 West Chapman Avenue*Recorded by LSA Associates, Inc. *Date: January 2019 Continuation Update***B10. Significance:** (continued from page 4)

This building is not individually eligible for listing in the National Register or California Register under these criteria, nor does it qualify as a contributor to the Old Towne Orange Historic District under Criteria B/2.

Under Criteria C/3, this building has not been identified as the work of a master and it does not possess high artistic values. The building retains characteristics of a 1920s One-Part Commercial block auto dealership, such as its brick construction and symmetrical façade with simple decorative elements (medallions, stringcourse, and urns) and large showroom windows, but it no longer has a rectangular plan, which is a key characteristic of the type. The façade retains a high degree of integrity, but there are additions to the east, west, and north elevations. In addition, in the east elevation three large windows have been removed/filled in and a modern door has been installed and in the west elevation a bay door has been partitioned to include a pedestrian door. Unlike many One-Part Commercial Block buildings, which front on one street and only have one or two visible elevations, this building is adjacent to three streets and each elevation and all of its additions and alterations is visible. Therefore, the compromised integrity of design, materials, and workmanship on the secondary elevations makes a greater negative impact than they normally might. Due to this lack of integrity, the building is not a true representative of a One-Part Commercial block auto dealership. It is not an excellent example of an architectural style or architect's work with few or no alterations.

This building is not individually eligible for listing in the National Register or California Register under Criteria C/3. However, the original building (excluding all three additions) remains a contributor to the Old Towne Orange Historic District. It was constructed during the District's period of significance (1888–1940) and contributes to the wide range of property types within the District.

Under Criteria D/4, the building and related additions were constructed in the 20th century using common construction materials and methods. There is no indication that they have any potential to yield information important in prehistory or history. No archaeological resources were identified in the project area. The building is not eligible for listing in the National Register or California Register under criteria D/4 and does not contribute to the Old Towne Orange Historic District under these criteria.

***B12. References:** (continued from page 2)

Ancestry.com

- Var. A variety of records were accessed online in November 2017 and April and May 2018 at: <http://home.ancestry.com/>. These include city directories, voter registration records, and United States Census Data.

Brigandi, Phil

- 2017 Email correspondence with Mr. Brigandi, noted local historian, regarding the history of the project area and Harry D. Riley.

Chattel Architecture

- 2005 Department of Parks and Recreation form for Chapman W. 401, APN 039-171-12. Provided by the City of Orange.

City of Orange

- Var. Building permits for 305 South Main Street. On file at the City of Orange, Building Division.

Los Angeles Times

- 1924 Building in Orange on Up Grade. March 8, page 13.
 1925a Four Secret Prisoners in Anaheim Klan War. January 27, pages 1 and 2.
 1925b Klan Scoured in Resolution. January 13, page 10.
 1925c Anti-Klan Forces Lead in Election at Anaheim. February 4, pages 1 and 3.
 1959 Advertisement. Put a Boat in Your Budget. July 26, page 209 (third column).
 1966 Advertisement. May 29, page 309.
 1969 Advertisement. November 22, page 94.

Orange County Archives

- Var. Email correspondence in March 2021 that resulted in the Archives providing city directory information regarding various addresses in and around the project area.

Sanborn Fire Insurance Maps

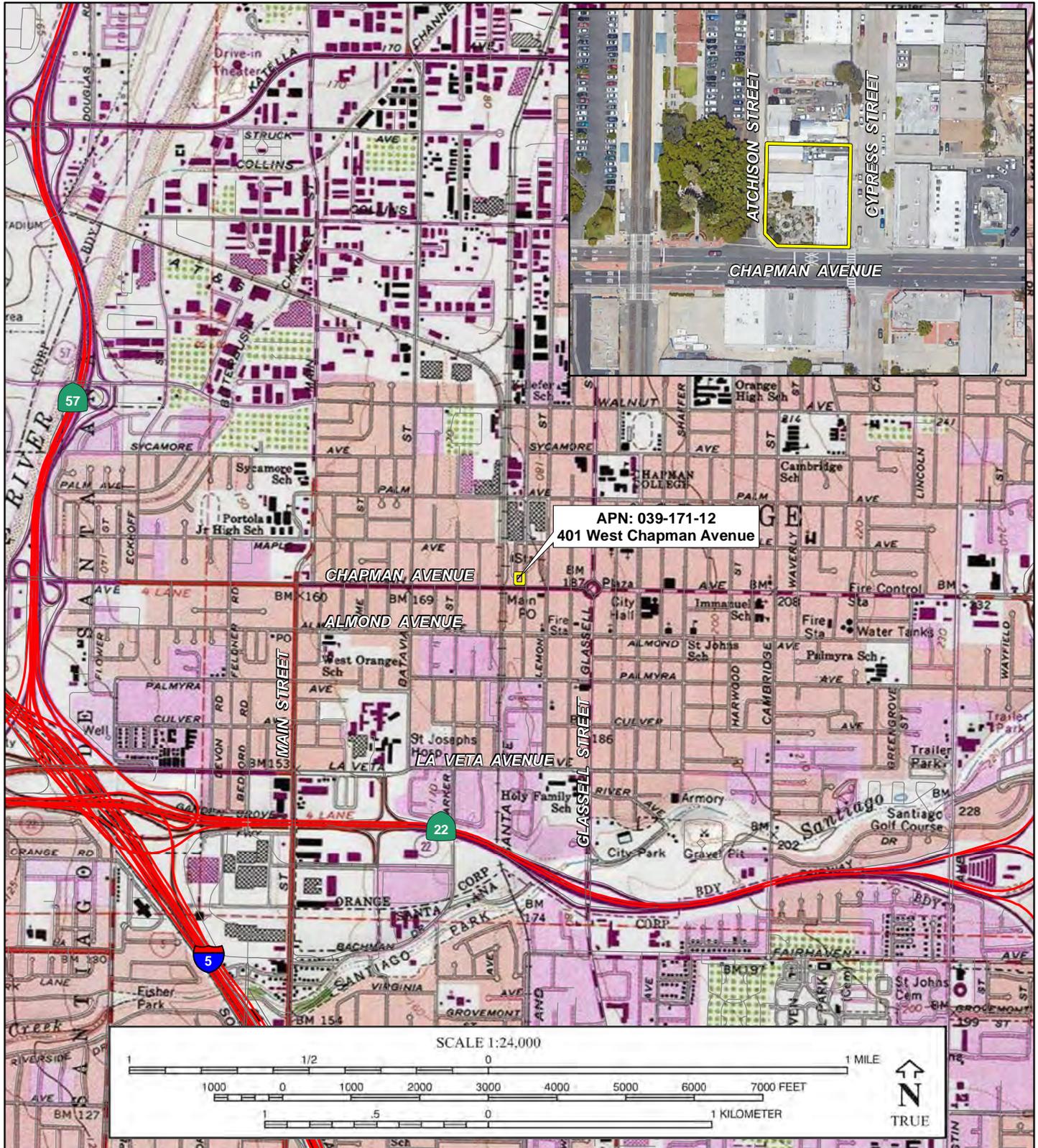
- 1950 Accessed online in November 2017 and 2018 via the Los Angeles Public Library's website at: <https://www.lapl.org/>.

Santa Ana Register

- 1919 Longworth Sells His Garage at Anaheim. August 9, page 9.
 1920a Anaheim Board to Advertise Section. January 14, page 6.
 1920b Anaheim Boost Clubs to Unite. June 15, page 6.
 1923 Orange Firm to Put Up Building. October 11, page 17.
 1929a Year Seen as Banner Period for Building. January 3, page 7.
 1929b You and Your Friends. July 22, page 11.
 1929c Harry D. Riley Announces Concentration of Business and Civic Interest in S. A. August 4, page 13.
 1930 H. Doty Buys Interest in Car Company. July 20, page 10.

State of California - Resource Agency
DEPARTMENT OF PARKS AND RECREATION
LOCATION MAP

Primary # _____
 HRI # _____
 Trinomial _____



State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # 30-159847
HRI # 039213
Trinomial ORA
NRHP Status Code 1D

Other Listings:
Review Code:

Reviewer:

Date:

Page 1 of 3

*Resource Name or #:
(Assigned by Recorder)

CHAPMAN_W_401__APN_039-171-12

P1. Other Identifier: Same building as: 112 N. Cypress.

*P2. Location: Not for Publication Unrestricted

*a. County: Orange and (P2b and P2c or P2d. Attach a location map as necessary.)

*b. USGS 7.5' Quad: Date: T ; R ; 1/4 of 1/4 of Sec ; B.M.

c. Address: 401 - W CHAPMAN AVE , # City: Orange Zip: 92866

d. UTM: (Give more than one for large and/or linear resources) Zone mE/ mN

e. Other Locational Data: Same APN as: 107 N. Atchison and 403 W. Chapman.

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries. Continues on Pg.3.)

Materials: Masonry - Brick

Brick Commercial. As typical with commercial buildings of this era, the lower portion of the store front is devoted almost exclusively to showcase windows. The double door entry features wood doors with a single, plate glass window and transom above. Transom windows were located across all of the windows on the lower portion of the building, but signs have since been placed in these transoms. Horizontal banding distinguishes

*P3b. Resource Attributes: (HP8)--Industrial building
(List attributes and codes)

*P4. Resources Present: Building Structure Object Site Element of District District Other (Isolates, etc.)

P5b. Description of Photo: 2005
(View, date, accession #)



*P6. Date Constructed/ Age and Source:

1920 c

Historic Prehistoric Both

*P7. Owner and Address:

*P8. Recorded by: (Name, affiliation, and address)

D. Gest, P. LaValley, D. Matsumoto

Chattel Architecture
13417 Ventura Blvd.
Sherman Oaks, CA 91423

*P9. Date Recorded:

May, 2005

*P10. Survey Type: (Describe)

Reconnaissance

*P11. Report Citation: (Cite survey report and other sources, or enter "none.")

Orange County Assessor Records (2005). Chattel Architecture (2005) Historic Resources Survey. AEGIS (1991) Historic Building Inventory Update. Heritage Orange County, Inc. (1982) Orange Historic Survey.

*Attachments: NONE Location Map Continuation Sheet(s) Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List):

DPR 523A (1/95)

*Required Information

Page 2 of 3

*Resource Name or #:
(Assigned by Recorder)

CHAPMAN_W_401__APN_039-171-12

B1. Historic Name: Schwab Chevrolet - Auto Sales and Repair

B2. Common Name:

B3. Original Use: IND B4. Present Use: IND

*B5. Architectural Style: Industrial

*B6. Construction History: (Construction date, alterations, and date of alterations) Date of Construction: 1920 c Historic Prehistoric Both

*B7. Moved? No Yes Unknown Date: Original Location:

*B8. Related Features:

*B9. Architect or Builder: Unknown

*B10. Significance: Theme: Architecture Area: City of Orange Property Type: Commercial

Period of Significance: Old Towne: Early Settlement (c. 1870 - 1920)

Applicable Criteria: AC

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity. Continues on Pg.4.)

Structural Integrity: Good Condition - No apparent change to original structure.

Site Integrity:

Opportunities: Appears to be a contributor to a district that appears eligible for local listing or designation: Railroad/Packing House NCA.

Part of Schwab Chevrolet, which encompassed the north-side of the 400 block of W. Chapman, including: the auto sales building with repair facilities in rear (403, W. Chapman), the auto repair building (401 W. chapman), and the auto wash building (107 N. Atchison). Past owners/occupants: 1927-1928 - Schwab Chevrolet dealers (P.E. Schwab); 1929 - Sackett-Doty Co. autos; 1932 - H. Vernon Doty Chevrolet; 1940 - Mansur Motors; and 1949-1950 - Mansur Motors.

B11. Additional Resource Attributes: (List attributes and codes)

*B12. References:

Orange Daily News.

B13. Remarks: (Continues on Pg.3.)

Status change since 1991 Survey: None.

Style previously noted in 1991 Survey as: Commercial.

(Sketch Map with North arrow required.)

*B14. Evaluator: Robert Chattel

*Date of Evaluation: September, 2005

(This space reserved for official comments.)

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # 30-159847
HRI # 039213
Trinomial ORA

Page 3 of 3

*Resource Name or #:
(Assigned by Recorder)

CHAPMAN_W_401__APN_039-171-12

Recorded by:

D. Gest, P. LaValley, D. Matsumoto
Chattel Architecture
13417 Ventura Blvd.
Sherman Oaks, CA 91423

Date Recorded: May, 2005

Continuation Update

Years Surveyed: 1982, 1991, 2005

Description of Photo: 1991

Listed in National Register: 1997

General Plan: OTMIX-15 # of Buildings: 1

Planning Zone: SP # of Stories: 1

Lot Acre: 0.4436 # of Units: 1

Principal Building Sqft: 6193

B6. Construction History (Continued from Pg.2):

B13. Remarks (Continued from Pg.2):

P3a. Description (Continued from Pg.1):

shes the cornice. Since its construction, this single-story brick building has been used for commercial purposes.



State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # 30-159847
HRI # 039213
Trinomial ORA
NRHP Status Code 1D

Other Listings: _____

Review Code: _____

Reviewer: _____

Date: _____

Page 1 of 3

*Resource Name or #:
(Assigned by Recorder)

CYPRESS_N_112__APN_039-171-12

P1. Other Identifier: Same building as: 401 W. Chapman.

*P2. Location: Not for Publication Unrestricted

*a. County: Orange and (P2b and P2c or P2d. Attach a location map as necessary.)

*b. USGS 7.5' Quad: _____ Date: _____ T _____ ; R _____ ; 1/4 of _____ 1/4 of Sec _____ ; B.M. _____

c. Address: 112 - N CYPRESS ST, # _____ City: Orange Zip: 92866

d. UTM: (Give more than one for large and/or linear resources) Zone _____ ' _____ mE/ _____ mN

e. Other Locational Data: Same APN as: 107 N. Atchison and 403 W. Chapman.

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries. Continues on Pg.3.)

Materials: Masonry - Brick

Small industrial building attached to the rear of the 1920's brick building at 401 W. Chapman Ave.

*P3b. Resource Attributes: (HP8)--Industrial building
(List attributes and codes)

*P4. Resources Present: Building Structure Object Site Element of District District Other (Isolates, etc.)



P5b. Description of Photo: 2005

(View, date, accession #)

*P6. Date Constructed/ Age and Source:

1920 c

Historic Prehistoric Both

*P7. Owner and Address:

*P8. Recorded by: (Name, affiliation, and address)

D. Gest, P. LaValley, D.
Matsumoto

Chattel Architecture
13417 Ventura Blvd.
Sherman Oaks, CA 91423

*P9. Date Recorded:

May, 2005

*P10. Survey Type: (Describe)

Reconnaissance

*P11. Report Citation: (Cite survey report and other sources, or enter "none.")

Orange County Assessor Records (2005). Chattel Architecture (2005)
Historic Resources Survey. AEGIS (1991) Historic Building Inventory
Update. Heritage Orange County, Inc. (1982) Orange Historic Survey.

*Attachments: NONE Location Map Continuation Sheet(s) Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

DPR 523A (1/95)

*Required Information

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # 30-159847
HRI # 039213
Trinomial ORA

Page 3 of 3

*Resource Name or #:
(Assigned by Recorder)

CYPRESS_N_112__APN_039-171-12

Recorded by:

D. Gest, P. LaValley, D. Matsumoto
Chattel Architecture
13417 Ventura Blvd.
Sherman Oaks, CA 91423

Date Recorded: May, 2005

Continuation Update

Years Surveyed: 1982, 1991, 2005

Description of Photo: 1991

Listed in National Register: 1997

General Plan: OTMIX-15 # of Buildings: 1

Planning Zone: SP # of Stories: 1

Lot Acre: 0.4436 # of Units: 1

Principal Building Sqft: 6193

B6. Construction History (Continued from Pg.2):

B13. Remarks (Continued from Pg.2):

P3a. Description (Continued from Pg.1):

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial ORA
NRHP Status Code 6Z

Other Listings: _____

Review Code: _____

Reviewer: _____

Date: _____

Page 1 of 3

*Resource Name or #:
(Assigned by Recorder)

CHAPMAN_W_403__APN_039-171-12

P1. Other Identifier: _____

*P2. Location: Not for Publication Unrestricted

*a. County: Orange and (P2b and P2c or P2d. Attach a location map as necessary.)

*b. USGS 7.5' Quad: _____ Date: _____ T _____ ; R _____ ; 1/4 of _____ 1/4 of Sec _____ ; B.M. _____

c. Address: 403 - W CHAPMAN AVE, # _____ City: Orange Zip: 92866

d. UTM: (Give more than one for large and/or linear resources) Zone _____ ' _____ mE/ _____ mN

e. Other Locational Data: Same APN as: 107 N. Atchison, 401 W. Chapman and 112 N. Cypress.

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries. Continues on Pg.3.)

Materials: Frame - Stucco or plaster & metal siding

Small industrial building attached to the rear of the 1920's brick building at 401 W. Chapman Ave..

*P3b. Resource Attributes: (HP6)--Commercial building
(List attributes and codes)

*P4. Resources Present: Building Structure Object Site Element of District District Other (Isolates, etc.)



P5b. Description of Photo: 2011

(View, date, accession #)

*P6. Date Constructed/ Age and Source:

Historic Prehistoric Both

*P7. Owner and Address:

*P8. Recorded by: (Name, affiliation, and address)

D. Gest, P. LaValley, D.
Matsumoto

Chattel Architecture
13417 Ventura Blvd.
Sherman Oaks, CA 91423

*P9. Date Recorded:

May, 2005

*P10. Survey Type: (Describe)

Reconnaissance

*P11. Report Citation: (Cite survey report and other sources, or enter "none.")

Orange County Assessor Records (2005). Chattel Architecture (2005)
Historic Resources Survey. AEGIS (1991) Historic Building Inventory
Update.

*Attachments: NONE Location Map Continuation Sheet(s) Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

DPR 523A (1/95)

*Required Information

*Resource Name or #: CHAPMAN_W_403__APN_039-171-12
 (Assigned by Recorder)

B1. Historic Name: Unknown

B2. Common Name: _____

B3. Original Use: COM B4. Present Use: COM

*B5. Architectural Style: Contemporary

*B6. Construction History: (Construction date, alterations, and date of alterations) Date of Construction: _____ Historic Prehistoric Both

*B7. Moved? No Yes Unknown Date: _____ Original Location: _____

*B8. Related Features: _____

*B9. Architect or Builder: Unknown

*B10. Significance: Theme: Architecture Area: City of Orange Property Type: Commercial

Period of Significance: Located in Old Towne Applicable Criteria: N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity. Continues on Pg.4.)

Structural Integrity: Good Condition - Low level of alteration.

Site Integrity: _____

Opportunities: Appears to be a non-contributor to a district that appears eligible for local listing or designation: Railroad/Packing House NCA.

B11. Additional Resource Attributes: (List attributes and codes) _____

*B12. References:
Orange Daily News.

B13. Remarks: (Continues on Pg.3.)
Status change since 1991 Survey: None.

(Sketch Map with North arrow required.)

***Required Information**

*B14. Evaluator: Robert Chattel

*Date of Evaluation: September, 2005

(This space reserved for official comments.)

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____

HRI # _____

Trinomial ORA _____

Page 3 of 3

*Resource Name or #:
(Assigned by Recorder)

CHAPMAN_W_403__APN_039-171-12

Recorded by:

D. Gest, P. LaValley, D. Matsumoto
Chattel Architecture
13417 Ventura Blvd.
Sherman Oaks, CA 91423

Date Recorded: May, 2005

Continuation Update

Years Surveyed: 1991, 2005

Description of Photo: 1991

Listed in National Register: _____

General Plan: OTMIX-15 # of Buildings: 1

Planning Zone: SP # of Stories: 1

Lot Acre: _____ # of Units: 1

Principal Building Sqft: _____

B6. Construction History (Continued from Pg.2):

B13. Remarks (Continued from Pg.2):

P3a. Description (Continued from Pg.1):

HISTORICAL INFORMATION

- 14. Construction Date(s) c1920 Original location Date moved
- 15. Alterations & date see Description (7)
- 16. Architect Builder
- 17. Historic attributes (with number from list)

SIGNIFICANCE AND EVALUATION

- 18. Context for Evaluation: Theme Architecture Area
Period Property Type Context formally developed?
- 19. Briefly discuss the property's importance within the context. Use historical and architectural analysis as appropriate. Compare with similar properties.

20. Sources

O. C. Assessor records.

21. Applicable National Register criteria

22. Other Recognition:

State Landmark Number

23. Evaluator AEGIS

Date of Evaluation 1990

24. Survey type

25. Survey name

26. Year Form Prepared 1982

By(Name)

Organization Heritage Orange County, Inc.

Address 206 W. Fourth St. Suite 316

City & Zip Santa Ana, CA 92701

Phone (714) 835 7287



(1168) 401 West Chapman Year Built: circa 1920 Style: Brick Commercial

Distinguishing Features: This is a single-story building of brick and originally and still used for commercial purposes. As typical with the commercial buildings of this era, the lower portion of the store front is devoted almost exclusively to showcase windows. The double door entry features wood doors with a single plate glass window and transom window above. Transom windows were located across all of the windows on the lower portion of the building, but signs have since been placed in these transoms. Horizontal banding distinguishes the cornice.

History: No notable findings to date.

(1169) 402 West Chapman Year Built: 1923 Style: Twenties-era Commercial

Distinguishing Features: This building has been determined eligible for the National Register. Please see individual listing.

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 6Z

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 4

Resource Name or #: 107 North Atchison Street

P1. Other Identifier: _____

***P2. Location:** Not for Publication Unrestricted ***a. County:** Orange and (P2b and P2c or P2d. Attach a Location Map as necessary.)

***b. USGS 7.5' Quad:** Orange, CA **Date:** 1981 **T** 4S; **R** 9W; **S.B.B.M.**

c. Address: 107 North Atchison Street **City:** Orange **Zip:** 92886

d. UTM: Zone: 11; _____ mE/ _____ mN (G.P.S.)

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate): APN 039-171-12; building is located approximately 125 feet north of West Chapman Avenue, adjacent to the right-of-way on the east side of N. Atchison Street

***P3a. Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)
This vernacular free-standing garage was previously documented in 2005 and was assigned a California Historical Resources status code of 6Z. It is rectangular in plan and surmounted by a catslide gable roof sheathed with corrugated metal. It has narrow eaves and the exterior walls are clad with non-original manufactured wood siding. Aside from small attic vents, there are no openings on the east or west sides. The south elevation has two large garage doors. The north elevation was obscured from view. The building is in good condition, but the non-original wood siding has compromised its integrity.

***P3b. Resource Attributes:** (List attributes and codes) HP6-1-3 story commercial building

***P4. Resources Present:** Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photo or Drawing (Photo required for buildings, structures, and objects.)



See Continuation Sheet

P5b. Description of Photo: (View, date, accession #) West elevation, view to the east (11/20/18)

***P6. Date Constructed/Age and Sources:** Historic Prehistoric Both
Circa 1955 (Sanborn Fire Insurance Map 1950; Orange County Archives 2017)

***P7. Owner and Address:**
Leason F. Pomeroy, FAIA
LP3 Architecture, Inc.
158 North Glassell Street
Orange, California 92886

***P8. Recorded by:** (Name, affiliation, and address)
Casey Tibbet, M.A.
LSA Associates, Inc.
1500 Iowa Avenue, Suite 200
Riverside, California 92507

***P9. Date Recorded:** November 2018

***P10. Survey Type:** (Describe) Intensive-level CEQA compliance

***P11. Report Citation:** (Cite survey report and other sources, or enter "none.") Cultural Resource Assessment for the Old Towne Gateway Project, Assessor's Parcel Numbers 039-171-12 and 039-171-03, City and County of Orange, California, 2021. Prepared by LSA Associates, Inc.

***Attachments:** NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record Photograph Record Other (List):

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 4

*NRHP Status Code 6Z

*Resource Name or # (Assigned by recorder) 107 North Atchison Street

B1. Historic Name: _____

B2. Common Name: _____

B3. Original Use: garage/storage B4. Present Use: garage/storage

*B5. Architectural Style: vernacular

*B6. Construction History: (Construction date, alterations, and date of alterations)
No building permits were found for this building. Based on an examination of Sanborn Fire Insurance maps and historic aerial photographs, it was built sometime between 1950 and 1960.

*B7. Moved? No Yes Unknown Date: _____ Original Location: _____

*B8. Related Features: This building shares the property with a 1924 commercial building (401 W. Chapman Avenue).

B9a. Architect: Not found b. Builder: Not found

*B10. Significance: Theme: Post-World War II Development Area: City of Orange

Period of Significance: circa 1955 Property Type: Commercial Applicable Criteria: NA

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

This circa 1955 vernacular building is not eligible for listing in the National Register of Historic Places (National Register) or California Register of Historical Resources (California Register). The local ordinance does not include criteria for individual properties. The property is within the National Register-listed Old Towne Orange Historic District, but was built outside the District's period of significance and is not a contributor to it. It is not a historical resource for purposes of the California Environmental Quality Act (CEQA). See *Continuation Sheet*

Historic Context. Refer to the related report (see P11 on page 1).

B11. Additional Resource Attributes: (List attributes and codes)

*B12. References:

Orange County Archives
2017 Email correspondence in November 2017 that resulted in the Archives providing various information including 1938 and 1947 aerial photographs of the project area.

Sanborn Fire Insurance Maps
1950 Accessed online in November 2017 and 2018 via the Los Angeles Public Library's website at: <https://www.lapl.org/>.

B13. Remarks:

*B14. Evaluator: Casey Tibbet, M.A., LSA Associates, Inc., 1500 Iowa Avenue, Suite 200, Riverside, California 92507

*Date of Evaluation: November 2018

(Sketch Map with north arrow required.)
Refer to Location Map

(This space reserved for official comments.)

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____
HRI # _____
Trinomial _____

Page 3 of 4 *Resource Name or #: (Assigned by recorder) 107 North Atchison Street
*Recorded by LSA Associates, Inc. *Date: November 2018 Continuation Update

P5a. Photo or Drawing (continued from page 1)



South and east elevations, view to the northwest (11/20/18).

B10. Significance: (continued from page 2)

People/Businesses Associated with this Building. This building was constructed sometime after 1950 and appears to have replaced a free-standing canopy that was used as an auto wash. No information about the owners/occupants during the historic period was found. This address was not found in the available City Directories and an online newspaper search using the address did not find any matches.

Significance Evaluation. This building is being evaluated under the National Register and California Register criteria. Because the two sets of criteria are so similar, they are addressed together to avoid redundancy.

Under Criteria A/1, this post-World War II garage building is associated with automotive car culture in the second half of the 20th century. However, no evidence was found that it is associated with any significant contributions to that theme. Furthermore, its nondescript, temporally ambiguous appearance does not convey its association with the period when it was built. It is not individually eligible for listing in the National Register or California Register under Criteria A/1. It was built outside the period of significance (1888–1940) of the Old Towne Orange Historic District and does not contribute to the District's significance.

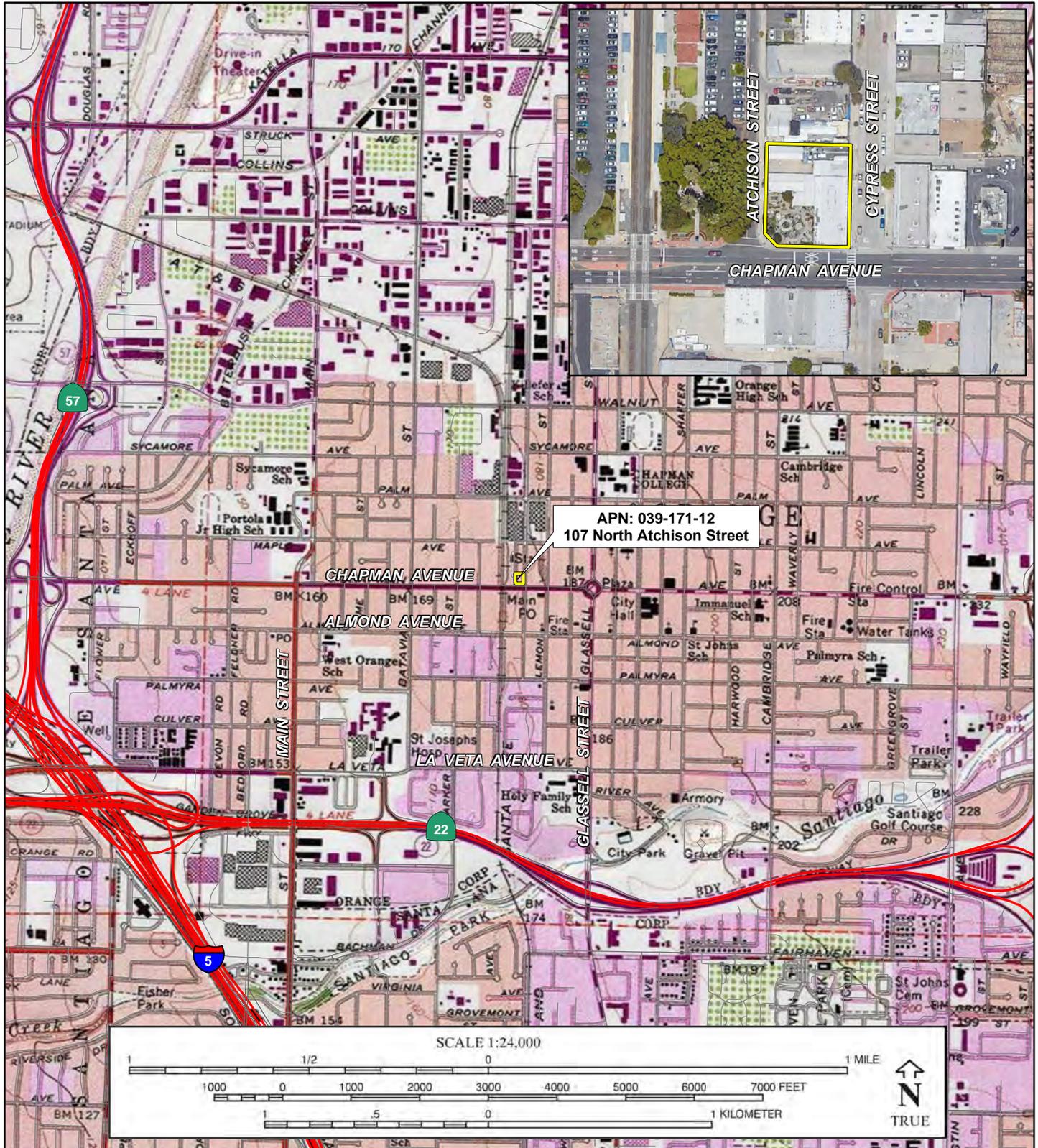
Under Criteria B/2, no information about the owners/occupants during the historic period was found. This building is not individually eligible for listing in the National Register or California Register under these criteria, nor does it qualify as a contributor to the Old Towne Orange Historic District under Criteria B/2.

Under Criteria C/3, this vernacular garage is nondescript and temporally ambiguous. There is no indication that it is the work of a master, it does not possess high artistic values, and it is not a true representative of a type, period, or method of construction. It is not individually eligible for listing in the National Register or California Register under these criteria and does not contribute to the significance of the Old Towne Orange Historic District under Criteria C/3.

Under Criteria D/4, the building was constructed circa 1955 using modern methods and materials and does not have the potential to yield important information. It is not eligible for listing in the National Register or California Register under Criteria D/4.

State of California - Resource Agency
DEPARTMENT OF PARKS AND RECREATION
LOCATION MAP

Primary # _____
 HRI # _____
 Trinomial _____





State of California - The Resources Agency
 DEPARTMENT OF PARKS AND RECREATION
 PRIMARY RECORD

Primary # _____

HRI # _____

Trinomial ORA _____

NRHP Status Code 6Z _____

Other Listings: _____

Review Code: _____

Reviewer: _____

Date: _____

Page 1 of 3

*Resource Name or #:
 (Assigned by Recorder)

ATCHISON_N_107_APN_039-171-12

P1. Other Identifier: _____

*P2. Location: Not for Publication Unrestricted

*a. County: Orange and (P2b and P2c or P2d. Attach a location map as necessary.)

*b. USGS 7.5' Quad: _____ Date: _____ T _____ ; R _____ ; 1/4 of _____ 1/4 of Sec _____ ; B.M. _____

c. Address: 107 - N ATCHISON ST, # _____ City: Orange Zip: 92866

d. UTM: (Give more than one for large and/or linear resources) Zone _____ ' _____ mE/ _____ mN

e. Other Locational Data: Same APN as: 112 N. Cypress and 401 & 403 W. Chapman.

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries. Continues on Pg.3.)

Materials: Frame - Wood siding

At this site and prior to this building, City directories listed the Park Hotel at 117 N. Atchison in 1925. As of 1930, the hotel had been demolished.

*P3b. Resource Attributes:
 (List attributes and codes)

*P4. Resources Present: Building Structure Object Site Element of District District Other (Isolates, etc.)

P5b. Description of Photo: 2005

(View, date, accession #)

*P6. Date Constructed/ Age and Source:

Historic Prehistoric Both

*P7. Owner and Address:

*P8. Recorded by: (Name, affiliation, and address)

D. Gest, P. LaValley, D. Matsumoto

Chattel Architecture

13417 Ventura Blvd.

Sherman Oaks, CA 91423

*P9. Date Recorded:

March, 2005

*P10. Survey Type: (Describe)

Reconnaissance



*P11. Report Citation: (Cite survey report and other sources, or enter "none.")

Orange County Assessor Records (2005). Chattel Architecture (2005) Historic Resources Survey.

*Attachments: NONE Location Map Continuation Sheet(s) Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

DPR 523A (1/95)

*Required Information

Page 2 of 3

*Resource Name or #:
(Assigned by Recorder)

ATCHISON_N_107__APN_039-171-12

B1. Historic Name: Schwab Chevrolet - Auto Wash

B2. Common Name: _____

B3. Original Use: IND

B4. Present Use: COM

*B5. Architectural Style: Industrial

*B6. Construction History: (Construction date, alterations, and date of alterations) Date of Construction: _____ Historic Prehistoric Both

*B7. Moved? No Yes Unknown Date: _____ Original Location: _____

*B8. Related Features: _____

*B9. Architect or Builder: Unknown

*B10. Significance: Theme: Architecture Area: City of Orange Property Type: Industrial

Period of Significance: Located in Old Towne

Applicable Criteria: N/A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity. Continues on Pg.4.)

Structural Integrity: _____

Site Integrity: _____

Opportunities: _____

B11. Additional Resource Attributes: (List attributes and codes) _____

*B12. References:

Orange Daily News. City Directories (1925). Sanborn Maps (1922, 1950). Arial photo (1938).

B13. Remarks: (Continues on Pg.3.)

Status change since 1991 Survey: Not previously surveyed.

(Sketch Map with North arrow required.)

*B14. Evaluator: Robert Chattel

*Date of Evaluation: September, 2005

(This space reserved for official comments.)

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____
HRI # _____
Trinomial ORA _____

Page 3 of 3

*Resource Name or #:
(Assigned by Recorder)

ATCHISON_N_107__APN_039-171-12

Recorded by:

D. Gest, P. LaValley, D. Matsumoto
Chattel Architecture
13417 Ventura Blvd.
Sherman Oaks, CA 91423

Date Recorded: March, 2005

Continuation Update

Years Surveyed: 2005

Description of Photo: 1991

Listed in National Register: _____

General Plan: OTMIX-15 # of Buildings: 1

Planning Zone: SP # of Stories: 1

Lot Acre: _____ # of Units: 1

Principal Building Sqft: _____

B6. Construction History (Continued from Pg.2):

B13. Remarks (Continued from Pg.2):

P3a. Description (Continued from Pg.1):

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 1D

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 5 Resource Name or #: 135 North Atchison Street

P1. Other Identifier: 125 Atchison Street (1950 and earlier)

***P2. Location:** Not for Publication Unrestricted ***a. County:** Orange and (P2b and P2c or P2d. Attach a Location Map as necessary.)

***b. USGS 7.5' Quad:** Orange, CA **Date:** 1981 **T4S; R9W; S.B.B.M.**

c. Address: 135 North Atchison Street **City:** Orange **Zip:** 92866

d. UTM: Zone: 11; _____ mE/ _____ mN (G.P.S.)

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate): APN 039-171-03; located on the east side of North Atchison Street just north of West Chapman Avenue.

***P3a. Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

This building was previously documented in 1991 and 2005. In 2005, it was assigned a California Historical Resources (CHR) status code of 3D (appears eligible for National Register of Historic Places [National Register] as a contributor to a National Register-eligible district through survey evaluation) (Chattel 2005). Based on the City's 2010 Historic Resources Inventory Update in 2012, the 2005 DPR form was amended to change the status code to 1D (contributor to a district or multiple resource property listed in the National Register by the Keeper). Listed in the California Register of Historical Resources (California Register).

This one-story Mission Revival style building is rectangular in plan and surmounted by a low-pitched, gable roof sheathed with corrugated metal and features Mission Revival style parapets on the east and west ends. The visible exterior walls are covered with stucco. The east elevation has a wood-framed, four-paned double-hung window and a wood-and-glass door, both of which appear to be original. The door and the window are each recessed in segmental arched openings accented with concrete "keystones." The door also has wide concrete trim. The north and south elevations of the Mission Revival style building have windows identical to the one in the east elevation (the north one is visible from inside the metal addition). The visible parts of the building are in good condition and retain a high degree of integrity. (See *Continuation Sheet*)

***P3b. Resource Attributes:** (List attributes and codes) HP9-Public utility building

***P4. Resources Present:** Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photo or Drawing (Photo required for buildings, structures, and objects.)



See Continuation Sheet

P5b. Description of Photo: (View, date, accession #) Façade, view to the northeast (11/20/18)

***P6. Date Constructed/Age and Sources:** Historic

Prehistoric Both
circa 1912 (Brigandi 2017)

***P7. Owner and Address:**

Leason F. Pomeroy, FAIA
LP3 Architecture, Inc.
158 North Glassell Street
Orange, California 92886

***P8. Recorded by:** (Name, affiliation, and address)

Casey Tibbet, M.A.
LSA Associates, Inc.
1500 Iowa Avenue, Suite 200
Riverside, California 92507

***P9. Date Recorded:**

November 2018

***P10. Survey Type:** (Describe) Intensive-level CEQA compliance

***P11. Report Citation:** (Cite survey report and other sources, or enter "none.") Cultural Resources Assessment for the Old Towne Gateway Project, Assessor's Parcel Numbers 039-171-12 and 039-171-03, City and County of Orange, California, 2021. Prepared by LSA Associates, Inc.

***Attachments:** NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List):

BUILDING, STRUCTURE, AND OBJECT RECORD

B1. Historic Name: _____

B2. Common Name: _____

B3. Original Use: transformer house **B4. Present Use:** storage

***B5. Architectural Style:** Mission Revival (altered)

***B6. Construction History:** (Construction date, alterations, and date of alterations)

No original building permits were found for this building. In order to determine the history of the building, a variety of sources was reviewed including, but not limited to, Sanborn Fire Insurance Maps, historic aerial photographs, newspapers, City Directories, and building permits. Sanborn maps from 1909 do not show this building on the property (Sanborn Fire Insurance Maps 1909). North Atchison Street was paved in 1912 and research indicates that the small Mission Revival style building was constructed about that time as a transformer house (*Orange Daily News* 1912; Brigandi 2017). The building is shown at 125 Atchison Street on the 1922 Sanborn maps (Sanborn Fire Insurance Maps 1922). The 1950 Sanborn map shows a one-story iron-on-frame storage building that wraps around the west and north sides of the original building, which was labeled as vacant at that time (Sanborn Fire Insurance Maps 1950).

***B7. Moved?** No Yes Unknown **Date:** _____ **Original Location:** _____

***B8. Related Features:**

B9a. Architect: Unknown **b. Builder:** Unknown

***B10. Significance: Theme:** Architecture **Area:** City of Orange

Period of Significance: circa 1912 **Property Type:** Industrial building **Applicable Criteria:** A and C

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

This altered circa 1912 Mission Revival style building does not retain enough integrity to be eligible for individual listing in the National Register or the California Register. However, largely due to the relative rarity of examples of the Mission Revival style in the Old Towne Orange Historic District, the original building (excluding the addition) remains a contributor to the Old Towne Orange Historic District. It is a historical resource as defined by the California Environmental Quality Act (CEQA).

Historic Context. Refer to the related report (see P11 on page 1).

B11. Additional Resource Attributes: (List attributes and codes)

***B12. References:**

- Brigandi, Phil
 - 2017 Email correspondence with Mr. Brigandi, noted local historian, regarding the history of the project area and Harry D. Riley.
- Chattel Architecture
 - 2005 Department of Parks and Recreation form for Chapman W. 401, APN 039-171-12. Provided by the City of Orange.
- Orange County Archives
 - Var. Email correspondence in March 2021 that resulted in the Archives providing city directory information regarding various addresses in and around the project area.

Orange Daily News
1912 Application for Permit. October 17, page 1.

Sanborn Fire Insurance Maps
1909 Accessed online in November 2017 and 2018 via the Los Angeles Public Library's website at: <https://www.lapl.org/>.
1922 Accessed online in November 2017 and 2018 via the Los Angeles Public Library's website at: <https://www.lapl.org/>.
1950 Accessed online in November 2017 and 2018 via the Los Angeles Public Library's website at: <https://www.lapl.org/>.

B13. Remarks:

***B14. Evaluator:** Casey Tippet, M.A., LSA Associates, Inc., 1500 Iowa Avenue, Suite 200, Riverside, California 92507
***Date of Evaluation:** January 2019

(This space reserved for official comments.)

(Sketch Map with north arrow required.)

Refer to Location Map

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____
HRI # _____
Trinomial _____

Page 3 of 5 *Resource Name or #: (Assigned by recorder) 135 North Atchison Street
*Recorded by LSA Associates, Inc. *Date: January 2019 Continuation Update

***P3a. Description** (continued from page 1)

The L-shaped corrugated metal addition wraps around the west (façade) and north sides of the Mission Revival style building completely obscuring these elevations of the original building. The addition appears to consist of two buildings, both with gable roofs with parapets sheathed with corrugated metal. There are openings on the south, west, and east sides. The addition is in poor condition and, judging from the patchwork of metal panels, has sustained various alterations. Based on what is visible of the original building through the windows, it is possible, that if revealed, the original elevations may retain at least a moderate degree of integrity.

P5a. Photo or Drawing (continued from page 1)



East and north elevations, view to the southwest (11/20/18)

***B10. Significance** (continued from page 2)

People/Businesses Associated with this Building. In 1922 and 1950, this building's address was 125 North Atchison Street. A search of City Directories using 125 and 135 North Atchison Street revealed three listings for the building between the early 1920s and mid-1960s. In 1924, the occupant was Orange County Renovators and in 1925 and 1965 it was listed as vacant (Orange County Archives var.). Online newspaper searches for both addresses did not return any matches. Previous documentation for this building indicates that in 1927–1928, the building was used as a repair shop for Schwab Chevrolet and that it continued to service subsequent dealerships at 401 and/or 403 West Chapman Avenue into the early 1990s (Chattel 2005). No additional information regarding historic-period owners/occupants was found.

Significance Evaluation. In compliance with City requirements, this building is being evaluated for significance under the National Register and California Register criteria. It is also being evaluated within the context of the Old Towne Orange Historic District.

Under Criteria A/1, this building was originally a transformer house and later an automotive repair shop. It is associated with the early industrial development of Orange and with early 20th century automotive culture. Because it has not functioned as a transformer house since at least the mid-1920s and all of the original equipment associated with its use as a transformer house has been removed, it does not convey its association with that function or early industrial development. As early as the mid-1920s, it was converted to an automotive repair shop and storage building. Sometime between 1938 and 1947, a large corrugated metal addition was added to the north and west elevations. The addition has obscured the original building and given it a nondescript, utilitarian appearance that does not convey an association with any particular function or period. Therefore, this building is not individually eligible for listing in the National Register or California Register under Criteria A/1 and does not contribute to the Old Towne Orange Historic District under these criteria.

Under Criteria B/2, very little information was found for owners/occupants of this building during the historic period and none appears to be historically important. This building is not individually eligible for listing in the National Register or California Register under these criteria, nor does it qualify as a contributor to the Old Towne Orange Historic District under Criteria B/2.

See Continuation Sheet

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____
HRI # _____
Trinomial _____

Page 4 of 5 *Resource Name or #: (Assigned by recorder) 135 North Atchison Street
*Recorded by LSA Associates, Inc. *Date: January 2019 Continuation Update

***B10. Significance** (continued from page 3)

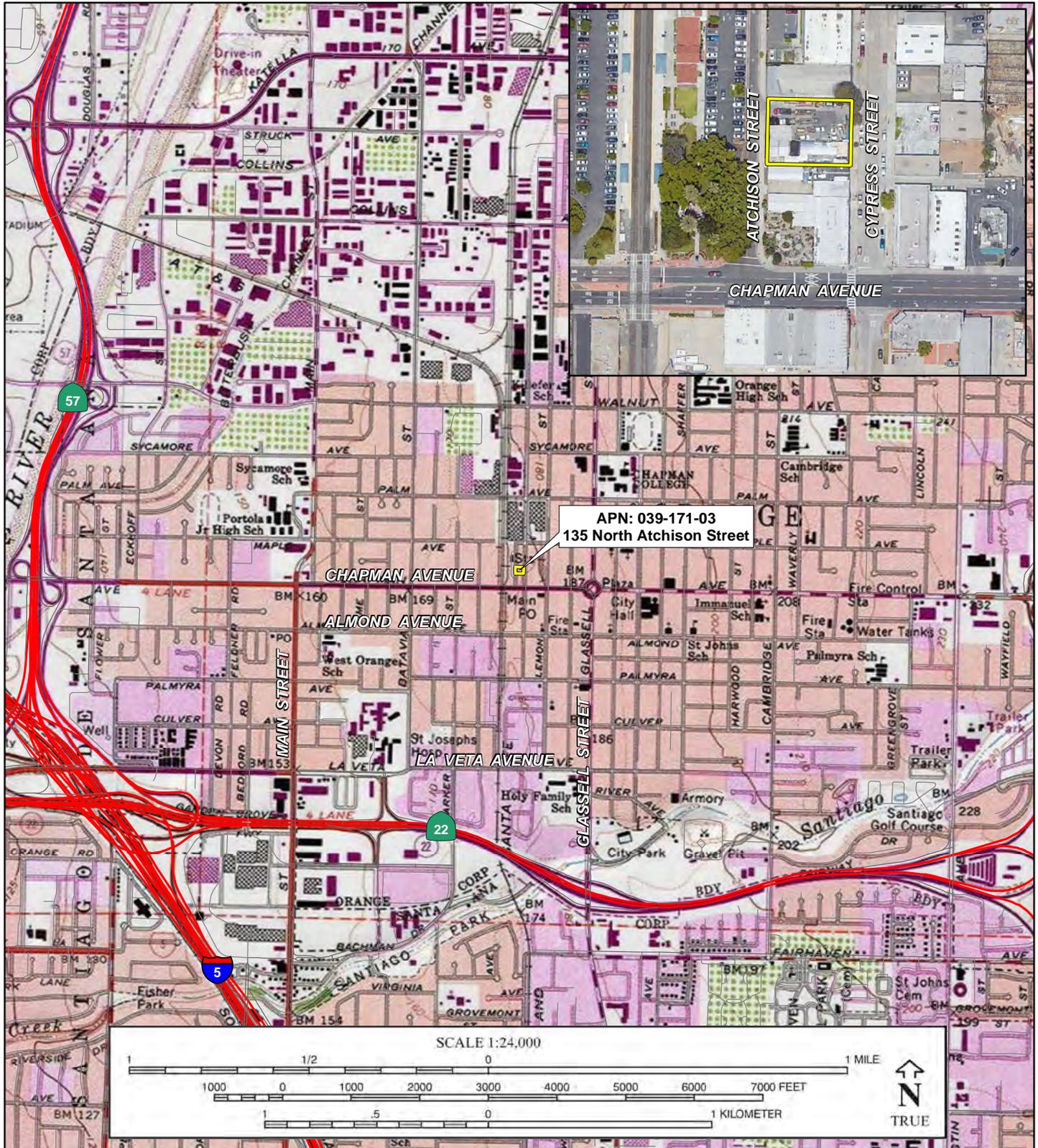
Under Criteria C/3, no information was found indicating this altered Mission Revival style building is the work of a master. The building has Mission Revival style parapets and segmental arched openings, but is modest in size, character, and design and does not possess high artistic values. While the south and east elevations retain a high degree of integrity, the north and west (street-facing façade) elevations are hidden behind a large, corrugated metal addition. It is unknown what the integrity level of the north and west elevations is behind the addition. Because of the large addition, the building is not a true representative of the Mission Revival style.

The building is not individually eligible for listing in the National Register or California Register under Criteria C/3. However, largely due to the relative rarity of examples of the Mission Revival style in the Old Towne Orange Historic District, the original building (excluding the addition) remains a contributor to the Old Towne Orange Historic District. It was constructed during the District's period of significance (1888–1940) and contributes to the wide range of architectural styles within the District.

Under Criteria D/4, the building is not an archaeological site and there is no indication that it has the potential to yield information important in prehistory or history. It was constructed circa 1912 using common methods and materials. The building is not eligible for listing in the National Register or California Register under these criteria and is not a contributor to the Historic District under these criteria.

State of California - Resource Agency
DEPARTMENT OF PARKS AND RECREATION
LOCATION MAP

Primary # _____
 HRI # _____
 Trinomial _____



State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary #

HRI #

Trinomial ORA

NRHP Status Code 3D

Other Listings:

Review Code:

Reviewer:

Date:

Page 1 of 4.1

*Resource Name or #:
(Assigned by Recorder)

ATCHISON_N_135_APN_039-171-03

P1. Other Identifier: See B13 Remarks

*P2. Location: Not for Publication Unrestricted

*a. County: Orange and (P2b and P2c or P2d. Attach a location map as necessary.)

*b. USGS 7.5' Quad: Date: T ; R ; 1/4 of 1/4 of Sec ; B.M.

c. Address: 135 - N ATCHISON ST, # City: Orange Zip: 92866

d. UTM: (Give more than one for large and/or linear resources) Zone mE/ mN

e. Other Locational Data: Same APN as a shared storage yard at: 136 N. Cypress.

*P3a. Description: *** Property status pending review with the National Register! ***

Materials: Frame - Stucco or plaster

The very nice Mission style building has been hidden from the street by an addition with a corrugated metal wall facing the street. The original building is a rectangle with a Mission style parapet on the east and west ends. The walls are a heavy texture concrete and there is a smooth concrete band along the top of the parapet. In the west wall, which is now an interior wall, there is a wide segmental arch opening with a

*P3b. Resource Attributes: (HP9)--Public utility transformer building
(List attributes and codes)

*P4. Resources Present: Building Structure Object Site Element of District District Other (Isolates, etc.)

P5b. Description of Photo: 2005
(View, date, accession #)



*P6. Date Constructed/ Age and Source:

1926 c

Historic Prehistoric Both

*P7. Owner and Address:

*P8. Recorded by: (Name, affiliation, and address)

D. Gest, P. LaValley, D. Matsumoto

Chattel Architecture
13417 Ventura Blvd.
Sherman Oaks, CA 91423

*P9. Date Recorded:

March, 2005

*P10. Survey Type: (Describe)

Reconnaissance

*P11. Report Citation: (Cite survey report and other sources, or enter "none.")

Orange County Assessor Records (2005). Chattel Architecture (2005) Historic Resources Survey. AEGIS (1991) Historic Building Inventory Update. Heritage Orange County, Inc. (1982) Orange Historic Survey.

*Attachments: NONE Location Map Continuation Sheet(s) Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List):

DPR 523A (1/95)

*Required Information

Page 2 of 4.1

*Resource Name or #:
(Assigned by Recorder)

ATCHISON_N_135__APN_039-171-03

B1. Historic Name: Schwab Chevrolet - Auto Repair Shop

B2. Common Name: _____

B3. Original Use: COM

B4. Present Use: COM

*B5. Architectural Style: Mission Commercial

*B6. Construction History: (Construction date, alterations, and date of alterations) Date of Construction: 1926 c Historic Prehistoric Both

Front façade and north wall have been covered with metal corrugated additions between 1922 and 1950. Building may be restored if additions removed.

***** Property status pending review with the National Register! *****

*B7. Moved? No Yes Unknown Date: _____ Original Location: _____

*B8. Related Features: _____

*B9. Architect or Builder: Unknown

*B10. Significance: Theme: Architecture Area: City of Orange Property Type: Transformer station

Period of Significance: Old Towne: Agriculture & Industry (c. 1880 - 1950) Applicable Criteria: AC

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity. Continues on Pg.4.)

Structural Integrity: Fair Condition - A collection of minor, inappropriate changes to original structure.

Site Integrity: Accessory structure or major addition that is appropriate to the site.

Opportunities: _____

The old Mission style building originally served as a transformer station (1922 Sanborn Maps), yet was quickly converted to a commercial use. The addition and remodel likely occurred during the use change. City Directories indicate it was the auto repair shop for the Schwab Chevrolet dealership in 1927-28, and continued to service the new dealers, Sackett-Doty Company in 1929, and Cruzen and Smith, and Mansur Motors in 1940. Past owners/occupants: (401-403 W. Chapman) 1927-1928 - Schwab Chevrolet dealers (P.E. Schwab); 1929 - Sackett-Doty Co. autos; 1930 - vacant; 1932 - H. Vernon Doty Chevrolet; 1940 - Mansur Motors (401 W. Chapman and Cruzen and Smith (402 W. Chapman); 1949-1950 - Mansur Motors (401 W. Chapman) and Chapman Guaranty Chevrolet (402 W. Chapman); 1991 - All City Import car dealer (403 W. Chapman). This s

B11. Additional Resource Attributes: (List attributes and codes) _____

*B12. References:

Orange Daily News. City Directories, L.A. County (1883-1884) and City of Orange Section (1901, 1907 - of the age 15 years and over). Orange Householder's Guide (1919, 1922-1930, 1932, 1940, 1949-1950). City Directory by Streets and the Classified Business (1919, 1922-1930, 1932). Specific Name in the Street Directory (1940). Sanborn Maps (1909, 1922, 1950, 1954). Plat Maps (1915-1920).

B13. Remarks: (Continues on Pg.3.)

Status change since 1991 Survey: None.

Previously known in 1991 Survey as: 109 N. Atchison. According to 1950 Sanborn Maps, 136 N. Cypress appears to have been a former building, which served as a machine shop, on the same lot.

*B14. Evaluator: Robert Chattel

*Date of Evaluation: September, 2005

(This space reserved for official comments.)

(Sketch Map with North arrow required.)

Page 3 of ##

*Resource Name or #:

ATCHISON_N_135__APN_039-171-03

Recorded by: *** Property status pending review with the National Register! ***

D. Gest, P. LaValley, D. Matsumoto
Chattel Architecture
13417 Ventura Blvd.
Sherman Oaks, CA 91423

Date Recorded: March, 2005

Continuation Update

Years Surveyed: 1982, 1991, 2005

Description of Photo: 1991

Listed in National Register: --

General Plan:	OTMIX-15	# of Buildings:	1
Planning Zone:	SP	# of Stories:	1
Lot Acre:		# of Units:	1
Principal Building Sqft:	1800		



B6. Construction History (Continued from Pg.2):

B13. Remarks (Continued from Pg.2):

P3a. Description (Continued from Pg.1):

trim band and keystone. There are two large wood sash, double-hung windows set in a segmental arch opening. These also have a keystone at the top. The same motif is repeated on the east wall in one wide door and single window. Another corrugated addition hides the north wall. The original building appears to be intact and could be restored with the removal of the additions.

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____

HRI # _____

Trinomial ORA _____

Page 4 of 4.1

*Resource Name or #:

ATCHISON_N_135__APN_039-171-03

Recorded by: ***** Property status pending review with the National Register! *****

D. Gest, P. LaValley, D. Matsumoto
Chattel Architecture
13417 Ventura Blvd.
Sherman Oaks, CA 91423

Date Recorded: March, 2005

Continuation Update

B10. Significance (Continued from Pg.2):

tructure is a good example of the architectural style.

State of California - The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
CONTINUATION SHEET

Primary # _____

HRI # _____

Trinomial ORA _____

Page 4.1 of 4.1

*Resource Name or #:

ATCHISON_N_135 APN_039-171-03

Recorded by: ***** Property status pending review with the National Register! *****

D. Gest, P. LaValley, D. Matsumoto
Chattel Architecture
13417 Ventura Blvd.
Sherman Oaks, CA 91423

Date Recorded: March, 2005

Continuation

Update

2012 National Register Amendment:

(Based upon 2010 Historic Resources Inventory Update)

Purpose of Amendment:

Not previously listed

Recommended Action: Amend contributor to NR District

Current Status (2011): _____

Recommended Status (2012): 1D

Detail: Resource located within Old Towne Orange National Register Historic District boundaries. Recommend District amendment to complete district inventory.

HISTORICAL INFORMATION

14. Construction Date(s) 1926A Original location same Date moved
15. Alterations & date reversable additions
16. Architect unknown Builder unknown
17. Historic attributes (with number from list) 06--Commercial Building

SIGNIFICANCE AND EVALUATION

18. Context for Evaluation: Theme Commercial Architecture Area City of Orange
Period 1874-1929 Property Type Context formally developed? no
19. Briefly discuss the property's importance within the context. Use historical and architectural analysis as appropriate. Compare with similar properties.

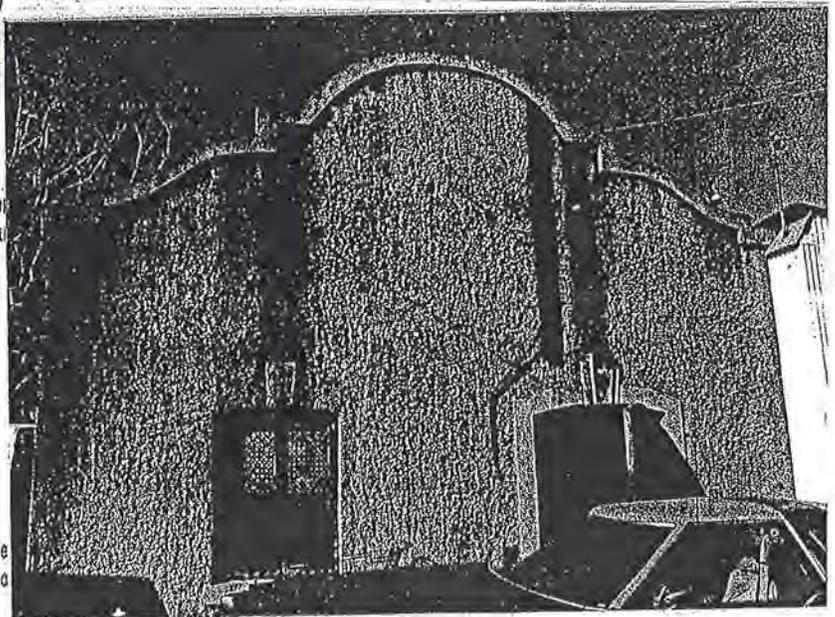
The old Mission style building at 109 N. Atchison is an autorepair structure. The research from the city directories and Sanborn Maps indicate it was built after 1922, and was the auto repair shop for the Schwab Chevrolet dealership in 1927-28, and continued to service the new dealers, Sackett-Doty Company in 1929, and Cruzen and Smith, and Mansur Motors in 1940.

The address has many discrepancies. The 1950 and 1954 Sanborn Maps show an auto repair building at 101-125 N. Atchison. In the city directories in 1925, a Park Hotel is listed at 117 N. Atchison, 1930, 117 N. Atchison is vacant. On the corrugated metal wall today, 107 N. Atchison is next to All City Import car dealership at 403 W. Chapman and 135 N. Atchison is After Image Auto Detail, although it is not listed in the phone directory today. The tax assessor records have no parcel numbers for N. Atchison in their records. The parcel number used is for 403 W. Chapman as the maps show the total area as one parcel.

This structure is a good example of the architectural style and would be a contributor to a Historic District and should be considered for nomination as a Local Landmark.

Past Owners/Occupants: (401-403 W. Chapman)

- 1925 Park Hotel (117 Atchison)
- 1926 vacant
- 1927-28 Schwab Chevrolet dealers (P. E. Schwab)
- 1929 Sackett-Doty Co. autos
- 1930 vacant, also vacant (117 Atchison)
- 1932 H. Vernon Doty Chevrolet
- 1940 Mansur Motors (401 W. Chapman)
- Cruzen and Smith (402 W. Chapman)
- 1949-50 Mansur Motors (401 W. Chapman)
- Chapman Guaranty Chevrolet (402 W. Chapman)
- 1991 All City Import car dealer (403 W. Chapman)



20. Sources

Orange County Assessor Records
City Directories, 1883-84 L.A. County and City of Orange
Section; 1901; 1907 of the age 15 years and over, also
the Classified Business.

Additional Directories including the Orange Householder's
Business: 1919; 1922-1930; 1932.

Orange Householder's Guide and specific names in the
Street Directory for 1940; Orange Householder's Guide
for 1949-50.

Sanborn Maps

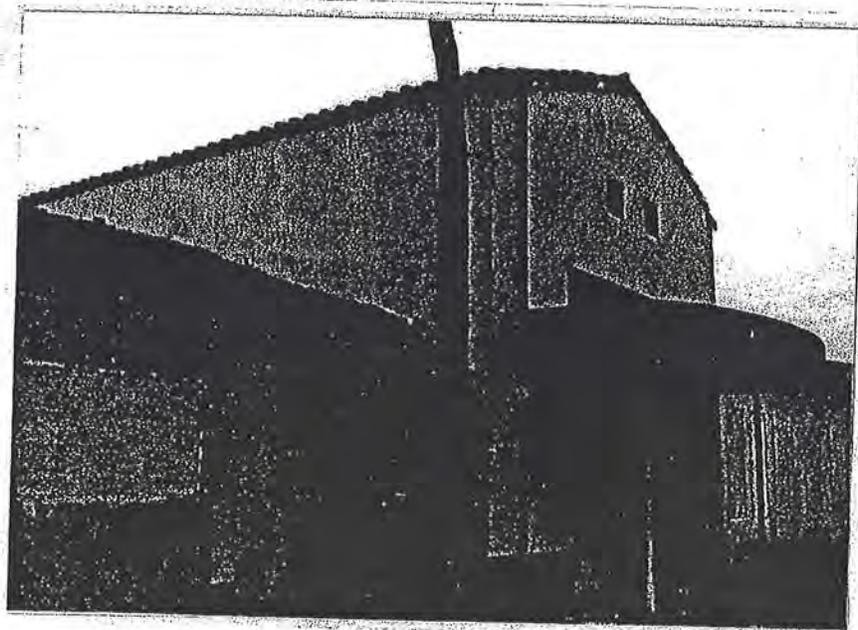
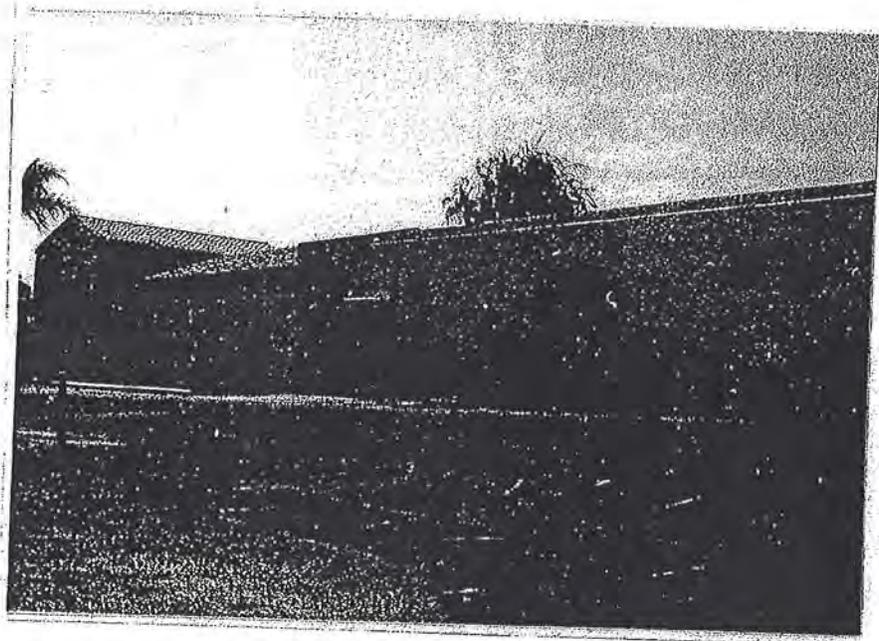
1909, 1922, 1950, 1954

Plat Maps

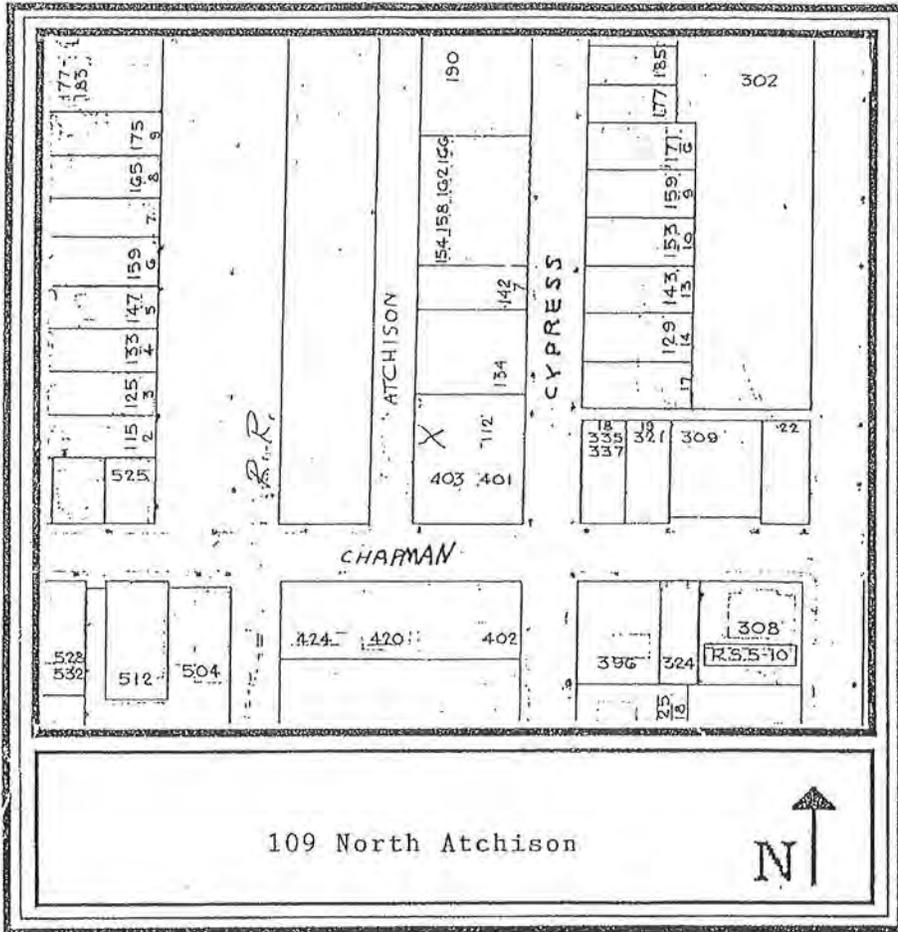
1915-1920

Guide, City Directory by Streets and the Classified

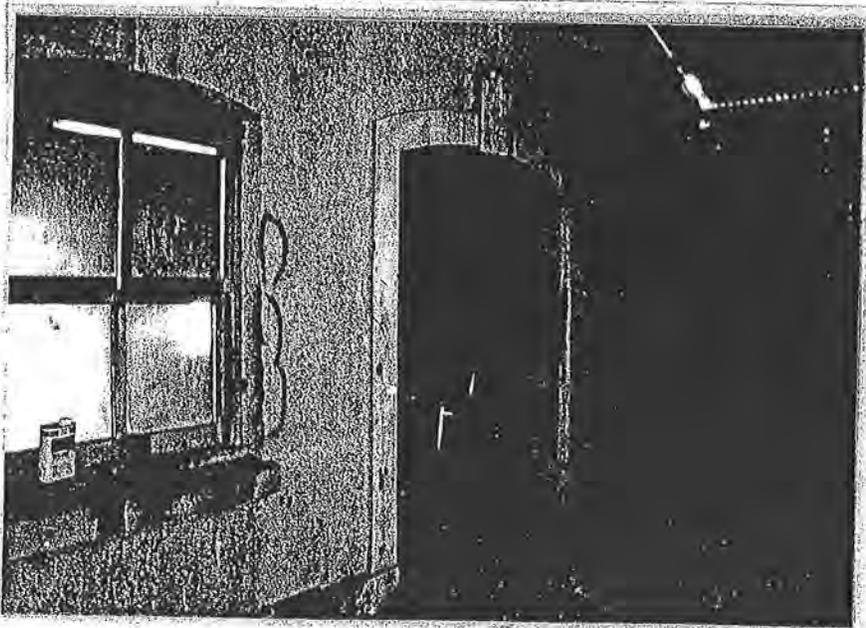
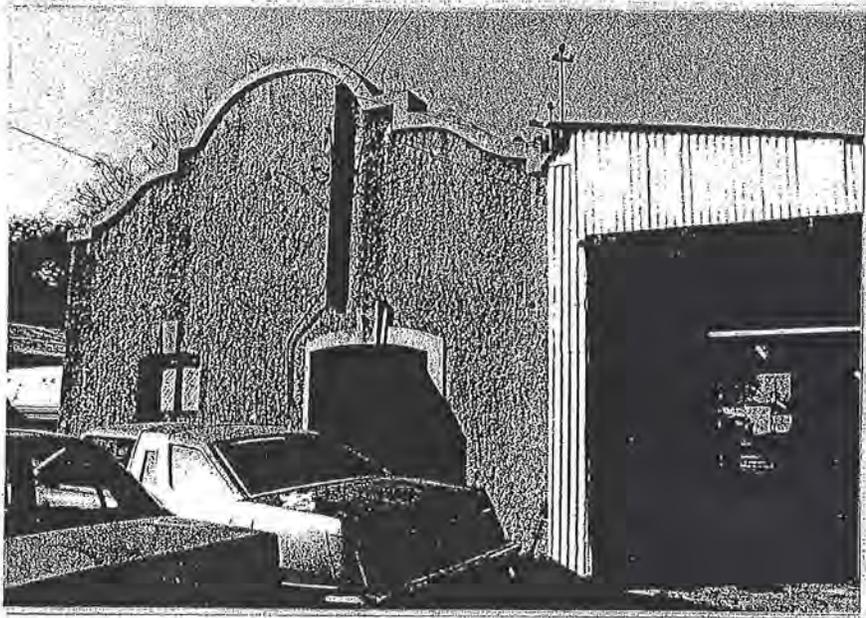
21. Applicable National Register criteria N/A
22. Other Recognition:
State Landmark Number
23. Evaluator
Date of Evaluation
24. Survey type
25. Survey name Orange Survey
26. Year Form Prepared 1991
By(Name)
Organization AEGIS
Address 111 SPRING STREET
City & Zip CLAREMONT, CA 91711
Phone (714) 621 1207



NW BK 1 D1



109 North Atchison



APPENDIX B

PROJECT PLANS

OLD TOWN GATEWAY

SHEET INDEX

ARCHITECTURE AND PLANNING

SHEET C1 - LETTER TO DRC / LETTER OF INTRO / PROJECT INFO
SHEET C2 - EXISTING SITE / DEMOLITION PLAN
SHEET C3 - NEIGHBORHOOD PLAN / EXISTING BUILDING 1
SHEET C4 - BUILDING 1 / CITY DESIGN CRITERIA
SHEET A1 - ILLUSTRATIVE SITE PLAN
SHEET A2 - DIMENSIONED SITE PLAN
SHEET A3 - SITE ROOF PLAN
SHEET A4 - SITE EXITING PLAN
SHEET A5 - CONCEPTUAL SITE LIGHTING PLAN
SHEET A6 - HISTORIC BUILDING 1 PLAN AND ELEVATIONS
SHEET A7 - HISTORIC BUILDING 1 ROOF PLAN / STREET ELEVATIONS
SHEET A8 - HISTORIC BUILDING 1 SECTIONS / LIGHT FIXTURES
SHEET A9 - BUILDING 2 PLANS ELEVATIONS AND SECTION
SHEET A10 - BUILDING 3 PLANS ELEVATIONS AND SECTION
SHEET A11 - HISTORIC BUILDING 4 AND BLDG 5 / STREET ELEVATIONS

LANDSCAPE DESIGN

MIXED-USE SUBMITTAL

SMART - JULY 27, 2020
SMART - SEPT 21, 2020
DRC (PRELIM) - OCT 20, 2020
DRC (RE-SUB) - DEC 17, 2020
SMART - JAN 26, 2021
DRC (PRELIM) - FEB 18, 2021

REVISED SUBMITTAL

SMART - DEC 11, 2021

OWNER:

OLD TOWNE GATEWAY, LLC
158 North Glassell St. 201
Orange, California

Leason F Pomeroy III
partner

DEFERRED SUBMITTALS
1. FIRE SYSTEM DESIGN
2. FIRE SINKLER SYSTEM
3. KITCHEN DESIGN EQUIP.

ARCHITECT:

LP3, Inc.
158 North Glassell St. 201
Orange, California

Leason F Pomeroy III, FAIA
architect

- All glass windows and glass doors at street level shall be glazed with "clear" glass.
- Height of new buildings 2, 3, and 5 shall not exceed the height of 28 feet. 5% of the roof area can exceed 28 feet in height.
- Provide London Plane street trees at 30 to 40 feet on center in 5 foot square tree grates.
- Elevator shall be of a size to satisfy "gurney" requirements.
- All habitable public spaces on the first level shall be and are accessible.
- No public restrooms shall be provided.
- The location of the Fire Department Connection and related valves is with the final location approved by the Fire Department.
- Street frontages for new buildings 2 and 3 shall be retail uses at the first level. Ground level retail spaces shall have a minimum of 25 foot depth and an average of 14 feet in clear ceiling height. Store front walls shall be a minimum of 75% clear glass for the length of the wall.
- The proposed project is within the Santa Fe Depot Specific Plan area. Eight (8) parking spaces are provided.
- All new trees shall be a minimum of 24 inch square boxes.
- Planters between parking areas and the street should have plant material maintained to screen vehicles at approximately 36 inches high.
- Key boxes and key switches (Knox devices) shall be provided where necessary to ensure that immediate access for firefighting, rescue and other emergency purposes is possible.
- All existing lots are required to be combined.
- A Tentative Parcel Map application is required.
- A dedication is required at the northeast corner of Chapman and Atchison to accommodate a standard ADA compliant access ramp.
- Ground immediately adjacent to foundations shall have a minimum slope of 2% for impervious and 5% for pervious surfaces.
- Inused driveways shall be removed and reconstructed with full height curb.
- An addressing plan is required.
- A Sewer Capacity Analysis is required.
- A Drainage Assessment Letter is required.
- Transportation System improvement Program fees will be required.
- A dedicated fire service with backflow prevention is required.

Streetscape Patterns
Existing Building 1 at P.L. where entry activity occurs
Proposed Buildings 2 & 3 at P.L. where entry activity occurs

Parking Area
Accessible access routes
Plan provides open access to all public areas from handicapped stall - see Site Plan

Drive Aisles
Drive Aisle two way per City Std 17.34 Figure 17.34.110.A standard 25 foot back-up space/drive aisle

Driveways
Driveways per City Standard (Type 1) Plan No. 115 two new driveways (curb cuts). Plan indicates removing and rebuilding sidewalk and curb at all nine (9) existing driveways

Driveways across street
Same Standard

Driveways adjacent to site
Same Standard

Drop-off Areas
No Drop-off area proposed at this time

Loading Zones
Standard 10 x 40 ft. loading zone proposed off of Atchison in onsite parking area

Location and Dimension Driveways, Streets
See Dimensioned Site Plan for this information

Parking Spaces
7 public parking +1 handicapped parking space = 8 spaces

Pedestrian Walkways
Throughout the project to include major east-west paseo

Street Dimensions
See Site Plan for this information

Utility Meters/Transformers
Existing - all to be upgraded with new transformer location
Proposed - see Site Plan for new transformer

Parking Information

Existing Building Area	- 10,117 sf	Parking Provided	0
Proposed Building Area	- 15,105 sf		
Added (N) Building Area	- 4,988 sf	Parking Required	20
Proposed/Provided Parking -			
Parking Spaces Short			8 12

- Options to make-up 12 short parking spaces**
1. Pay parking fee to City Parking Fund
 2. Trade for permanent "paseo" easement
 3. Provide spaces with shared parking agreement within required walking distance

Property Lines
See Site/First Floor Plan for Lot Lines (may require consolidation)

Proposed Density
See question above - "Density" FAR - 50

Public ROW Improvements
Driveways - Close (E) and add (N) per City Stds. See Site Plan

Sidewalks - To remain (remove/replace and patch as required)

Water Meters - All water meters to be new

Utility Boxes - All utility boxes to be new

Fire Hydrants - Existing hydrants to remain see Fire Master Plan no additional fire hydrants are anticipated

Street Lights - All Street Lights to remain

Traffic Control Devices - No (N) devices proposed

Public Transportation Stops - No Transportation Stops proposed

Site Information
Address - 401 West Chapman Avenue
Assessor's Parcel Number - Parcel 1: APN 039-071-03 Parcel 2: APN 039-171-12
Existing Entitlements - none to my knowledge
Lot Number - Lot 8 and the North 45 feet of Lot 9 and Lots 10, 11, and 12 and the South 5 feet of Lot 9 all in Block B
Tract Number - Halladay Tract, City of Orange

Trash Enclosures
Three covered trash enclosures are proposed with three 6 yard bins in each (one trash enclosure per Commercial/Retail building)

Zoning Designation
Existing - see Question "General Plan and Land Use Designation"
Proposed - same designation OTMU - 15

PROJECT INFORMATION (from "Plan Submittal Requirements")

Building Core Information
Construction Type VB
Occupancy Type A-2 and S-2
Existing to remain (E) un-reinforced masonry and wood
New (N) wood frame

Contact Information
Name Leason F Pomeroy III
Mailing Address 150 N. Glassell St., suite 201 Orange, CA 92866
Telephone Number (714) 771-8400
E-Mail Address LP3ARCH@aol.com

Density
Existing 10,117 sf/30,300 sf = 33%
Proposed 15,105 sf/30,300 sf = 50%

Easements
Existing To my knowledge there are none
Proposed To my knowledge there are none

Fences and Walls
Proposed north P.L. - six feet CMU fence / one hr. bldg. wall
east P.L. - none
south P.L. - six (6) feet 3 feet plaster / 3 feet glass
west P.L. - six (6) feet 3 feet plaster / 3 feet glass and six (6) feet steel pickets and gate

Fire Department
Sprinkled Buildings
(E) Building 1 and (N) Buildings 2 & 3 - (NFPA - 13) yes
(E) Building 4 (374 sf) and (N) Building 5 (416 sf) - no

Floor Area
Proposed
Building 1 (Hist) First Floor 5,000 sf
Addition (restrooms) 1,330 sf
6,330 sf
Mezzanine (new) -375 sf
6,705 sf

Building 2 (New) First Floor (Commercial/Retail) 3,470 sf
Building 3 (New) First Floor (Commercial/Retail) 4,140 sf
Building 4 (Hist) First Floor (Commercial/Retail) 374 sf
Building 5 (New) First Floor (Storage) -416 sf
Total Floor Area all Proposed Buildings 15,105 sf

Existing Buildings to remain and be rehabilitated
Building 1 (Hist) First Floor 5,000 sf
Building 4 (Hist) First Floor -374 sf
5,374 sf

Buildings to be demolished
Building 1 (Hist) Mezzanine 191 sf
Additions and Shed structures 4,552 sf
4,743 sf

Total Floor Area all Existing Buildings 10,117 sf

General Land Use Designation
Existing OTMIX - 15
Proposed OTMIX - 15

Label type of construction, size, use, height, and roof
See Site/Floor Plans and Sections for this information

Landscape Areas including hardscape and paving
Landscape areas (softscape) 2,727 sf - 9%
Hardscape areas (walks and courtyards) 8,484 sf - 28%
Paving (vehicle drive and parking areas) 4,242 sf - 14%

Proposed building Total First Floor Areas 14,730 sf - 49%

Total Site Area 30,300 sf - 100%

Land Use
Existing Commercial/Storage
Proposed (Mixed Use) Commercial Retail/Storage

Lot Size
Existing 30,300 sf
Proposed (same) 30,300 sf

Old Towne Projects
Architectural Style
Existing Historic Buildings - 1921 Brick Commercial
Proposed New Buildings - 2021 similar to (E) Historic

Building Alignment
Existing and New on P.L. - align (same - no setbacks)

Bulk and Mass
New references Existing in height, width and mass

Number of Stories
New - 1 story / Existing - one story with mezzanine

Roof Form and Height
New Buildings 2 & 3 have roofs similar to Hist. Building 1 with exposed truss roof structures
New Building 5 has flat roof matching Addition to Bldg 1

FREE-STANDING NON-CONTRIBUTING SHEDS OF 5,189 SF PROPOSED TO BE DEMOLISHED.

• **THE USE** - THE SITE IS CURRENTLY OCCUPIED BY A MIX OF USES TO INCLUDE ANTIQUE SALES AND STORAGE OF CONSTRUCTION EQUIPMENT AND MATERIALS.

• **ARCHITECTURE** - THERE ARE TWO (2) EXISTING ARCHITECTURAL STYLES (BUILDINGS TO BE REHABILITATED) ON THE SITE. HISTORIC BUILDING 1 IS A SIMPLE BOX SHAPE, FLAT WALLS WITH LARGE CLASS FAÇADE ON THE FRONTAGE AND SOLID MASS ON THE OTHER WALLS WITH A PARAPET (FLAT APPEARING) ROOF CONTAINING A GLASS CLEARSTORY. HISTORIC BUILDING 2 IS A VERY SMALL SIMPLE PLASTER BOX WITH A "MISSION INFLUENCED" FAÇADE - RELATING DIRECTLY TO THE DEPOT ACROSS ATCHISON STREET - WITH AN EXPOSED SLOPING SHEET METAL ROOF. THE BALANCE OF THE STRUCTURES ON THE SITE COMPRIZE ADDITIONS AND SHEDS OF PLASTER AND WOOD WITH "UTILITY" CHARACTER OF NO REDEEMING ARCHITECTURAL STYLE.

• **MATERIALS** - THE EXISTING BUILDINGS HAVE A VARIETY OF EXPOSED MATERIALS INCLUDING FACE BRICK, COMMON RED BRICK, PLASTER, WOOD, GLASS, PRE-CAST CONCRETE, AND SHEET METAL. THE PAVING IS A MIX OF ASPHALTIC MATERIALS AND CONCRETE.

• **COLORS** - HISTORIC BUILDING 1 HAS A MAJORITY OF LIGHT GRAY/WHITE FACE BRICK, RED COMMON BRICK, WITH BLACK PAINTED SHEET METAL AND WOOD TRIM. HISTORIC BUILDING 2 IS A MEDIUM GRAY PLASTER AND EXPOSED MEDIUM GRAY SHEET METAL ROOF.

• **TOPOGRAPHY** - THE SITE IS BASICALLY FLAT.

• **SCENIC VIEWS** - THE SITE HAS NO SCENIC VIEWS OTHER THAN VIEWS OF MEMORIAL PARK LANDSCAPE TO THE WEST.

• **SOILS** - THE SOIL IS SANDY WITH COBBLES HAVING A PRECULATION RATE OF 0.33 INCHES PER HOUR AT A DEPTH OF 39 FEET.

• **NATIVE LANDSCAPING/TREES** - THE SITE CONTAINS LITTLE OR NO EXISTING LANDSCAPING TO BE RETAINED OTHER THAN A LARGE PINE TREE LOCATED AT THE EXTREME NORTH-EAST CORNER OF THE SITE WHICH SUFFERED FROM A FIRE IN THE PAST, HAS BEEN POORLY MAINTAINED AND TRIMMED, AND IS PLANNED TO BE REMOVED ALONG WITH THE NON-CONTRIBUTING STRUCTURES

3. DESCRIBE THE SURROUNDING PROPERTIES, INCLUDING THE TYPE OF LAND USE, INTENSITY OF LAND USE, AND SCALE OF DEVELOPMENT.

• **NEIGHBORING LAND USES** - THE NEIGHBORING LAND USES ARE A MIX AND ARE IN THE TRANSITION FROM INDUSTRIAL/AUTO USES TO A MIX OF USES INCLUDING COMMERCIAL (RESTAURANTS/RETAIL), RESIDENTIAL, AND INSTITUTIONAL (UNIVERSITY USES).

• **INTENSITY** - THERE IS NO INTENSITY OF USE ON THE PRIVATELY OWNED PROPERTIES. THE ONLY INTENSE USE OF LAND IN THE NEIGHBORHOOD IS AFFORDED BY THE UNIVERSITY WHICH SEEMS TO BE INCREASING.

• **SCALE** - IT WOULD APPEAR THAT THE MAJORITY OF THE EXISTING NEIGHBORHOOD USES OF THE PRIVATE LAND ARE UNDER DEVELOPED REGARDING SCALE. MOST BUILDINGS ARE ONE STORY AND ZONING ALLOWS TWO STORIES THUS MORE SCALE MAY BE COMING IN THE FUTURE.

4. EXPLAIN ANY UNIQUE CONDITIONS ON THE SITE SUCH AS LOCATION, SHAPE AND/OR TOPOGRAPHY.

• **UNIQUE CONDITIONS** - THE SITE IS UNIQUE IN THAT IT CONTAINS TWO (2) HISTORIC BUILDINGS (TO BE REHABILITATED) AND ITS LOCATION BEING ACROSS ATCHISON STREET FROM MEMORIAL PARK AND THE MULTI-MODAL STATION (TRAIN STATION) AND IS NEAR THE NEW PUBLIC PARKING STRUCTURE AND THE PLAZA DISTRICT.

5. EXPLAIN ANY PHASING OF THE PROJECT

• **PROJECT PHASING** - IT IS NOT ANTICIPATED AT THIS TIME THAT THE CONSTRUCTION OF THE PROJECT WOULD BE PHASED.

6. ALCOHOL BEVERAGE CONTROL (ABC)

• **APPLICATION - AN (ABC) APPLICATION IS NOT PART OF THIS APPLICATION, BUT EVENTUALLY WILL BE REQUIRED.**

REQUIRED SUBMITTAL ITEMS (FROM "Plan Submittal Requirements")

PROJECT SCOPE AND ARCHITECTURAL DESIGN CONCEPT

1. **SCOPE** - THE PROPOSED PROJECT INCLUDES THE REHABILITATION OF TWO (2) HISTORIC BUILDINGS AND THE REPLACEMENT OF AN EXISTING ADDITION TO HISTORIC BUILDING 1. THE NEW ADDITION IS IN ARCHITECTURAL CONTRAST TO HISTORIC BUILDING 1. IT ALSO INCLUDES THE DEMOLITION OF ALL EXISTING ADDITIONS AND FREE-STANDING NON-CONTRIBUTING STRUCTURES, THE CONSTRUCTION OF TWO (2) NEW INDUSTRIAL APPEARING (COMMERCIAL/RETAIL) BUILDINGS AND A NEW STORAGE BUILDING MATCHING THE DESIGN OF THE BUILDING 1 ADDITION.

2. **ARCHITECTURAL DESIGN CONCEPT** - THE TWO (2) EXISTING HISTORIC BUILDINGS ARE TO BE REHABILITATED PER THE SECRETARY OF THE INTERIOR (SOI) STANDARDS FOR THE REHABILITATION OF HISTORIC BUILDINGS. THE TWO (2) NEW PROPOSED BUILDINGS ARE DESIGNED TO RESPOND TO THE SCALE AND MASS OF THE EXISTING HISTORIC BUILDINGS AS WELL AS REFERENCING MATERIALS AND DESIGN ELEMENTS FOUND IN THE HISTORIC BUILDINGS WHILE PROVIDING A DIFFERENTIATION IN STYLE PER SOI STANDARD NO. 3. THE TWO NEW BUILDINGS ARE DESIGNED WITH THE INTENTION OF THEIR ARCHITECTURAL STYLE BEING OF THE PRESENT TIME WHILE REFERENCING THE HISTORIC BUILDING 1. THE TWO NEW BUILDINGS USE THEIR SYMMETRICAL NEGATIVE SPACE TO FOCUS ON THE UNIQUE SMALLER HISTORIC BUILDING OF A COMPLETELY DIFFERENT STYLE (MEDITERRANEAN REVIVAL) WHICH IS RELATED TO THE MOST RECENT DEPOT BUILDING DESIGN. Per Douglas Westfall

IT IS THE PRIMARY INTENT OF THE DESIGN TO CELEBRATE THE REHABILITATED HISTORIC BUILDINGS (1920s) WITH THE DESIGN OF THE NEW BUILDINGS USING RELATED HISTORIC MATERIALS, SCALE AND MASS, WHILE BEING OF THEIR OWN TIME (2021c). THE ARCHITECTURE OF THE NEW BUILDINGS RELATES TO THE "INDUSTRIAL" BOX FORMS OF HISTORIC BUILDING 1 AS WELL AS TO MANY OF THE EXISTING HISTORIC BUILDINGS IN THE NEIGHBORHOOD.

LETTER OF EXPLANATION/JUSTIFICATION

1. TYPE OF BUSINESS, USES, HOURS OF OPERATION, VEHICLES OWNED, NUMBER OF EMPLOYEES, TYPE OF MATERIALS HANDLED, HISTORY OF BUSINESS.

• **TYPE OF BUSINESS** - THE TWO (2) HISTORIC BUILDINGS WILL HOUSE FOOD SERVICE USES. THE LARGER BUILDING (HISTORIC BUILDING 1) WILL HOUSE A LARGE SIT-DOWN RESTAURANT AND BAR TO INCLUDE OUTDOOR DINING. THE SMALLER HISTORIC BUILDING (HISTORIC BUILDING 4) WILL PROVIDE QUICK "GRAB AND GO" FOOD FOR PERSONS USING THE VARIOUS MODES OF TRAVEL AT THE NEARBY TRANSPORTATION CENTER.

• **USES** - IN ADDITION TO THE TWO HISTORIC BUILDINGS, THE PLAN INCLUDES TWO ADDITIONAL NEW BUILDINGS (BUILDINGS 2 AND 3) DESIGNED TO HOUSE COMMERCIAL/RETAIL USES. A NEW BUILDING 5 WILL BE STRICTLY STORAGE IN SUPPORT OF HISTORIC BUILDING 1.

• **HOURS OF OPERATION** - THE RESTAURANT USES WILL BE OPEN FOR LUNCH AND DINNER (SAY 10:00 AM - 10:00 PM). THE COMMERCIAL/RETAIL USES WILL OPERATE DURING NORMAL BUSINESS HOURS (SAY 9:00 AM - 7:00 PM).

• **VEHICLES OWNED** - IT IS NOT ANTICIPATED THAT COMPANY OWNED VEHICLES WILL BE PARKED ON THE SITE.

• **NUMBER OF EMPLOYEES** - IT IS ANTICIPATED THAT THE LARGE RESTAURANT (HISTORIC BUILDING 1) WOULD HAVE APPROXIMATELY TWENTY (20) EMPLOYEES DURING NORMAL OPERATING HOURS. HISTORIC BUILDING 2 WOULD HAVE TWO (2) EMPLOYEES AND DEPENDING ON THE ACTUAL USES IN THE TWO (2) COMMERCIAL/RETAIL SPACES IN THE NEW BUILDING 2 AND 3, EACH WOULD HAVE THREE (3) TO SAY TEN (10) EMPLOYEES DEPENDING ON THE USE.

• **TYPES OF MATERIALS HANDLED** - THE RESTAURANT USES WOULD HANDLE FROZEN AND FRESH/DRY FOOD PRODUCTS. IT IS NOT KNOWN AT THIS TIME WHAT THE ACTUAL USES WILL BE IN THE TWO (2) NEW BUILDINGS, THEREFORE THE MATERIALS HANDLED IN EACH ARE NOT KNOWN UNTIL LEASES ARE SIGNED.

• **HISTORY OF BUSINESS** - A RESTAURANT TENANT HAS BEEN IDENTIFIED FOR HISTORIC BUILDING 1. THE TENANT HAS BEEN IN BUSINESS FOR APPROXIMATELY TWENTY (20) YEARS AND HAS TWO OTHER VERY SUCCESSFUL LOCATIONS IN PROPERTIES OWNED BY THE APPLICANT.

2. DESCRIBE PHYSICAL CHARACTERISTICS OF THE PROPERTY INCLUDING ANY EXISTING STRUCTURES ON THE SITE, THE USE, THE ARCHITECTURE, MATERIALS, COLORS, TOPOGRAPHY, SCENIC VISTAS, SOILS, MATURE LANDSCAPING/TREES.

• **PHYSICAL CHARACTERISTICS OF THE SITE** - THE SITE IS CURRENTLY OCCUPIED BY A MIX OF LARGE LOW-END ANTIQUE ARTICLE SALES LOCATED IN THE HISTORIC BUILDING 1 AND THE ADJACENT FENCED YARD ALONG CHAPMAN AVENUE FRONTAGE. THE BALANCE OF THE SITE IS A MIX OF OPEN YARD STORAGE (CONSTRUCTION MATERIALS) AND ODD SHED STRUCTURES ALL ENCLOSED WITHIN AN OPAQUE CHAIN LINK FENCE.

• **EXISTING STRUCTURES** - THE SITE CONTAINS TWO (2) HISTORIC BUILDINGS. HISTORIC BUILDING 1 IS A 5,000 SF ONE STORY BRICK BUILDING (POORLY REINFORCED) WITH A 191 SF MEZZANINE. PER Douglas Westfall's "A TOUR OF OLD TOWNE ORANGE" THIS BUILDING IS LISTED AS "1920c brick commercial". THE SECOND HISTORIC BUILDING, HISTORIC BUILDING 2, IS A SMALL 374 SF ONE STORY BUILDING, "1924c mission influenced" ALSO LISTED. THE BALANCE OF THE EXISTING STRUCTURES INCLUDE ADDITIONS

Members of the Design Review Committee
City of Orange
300 East Chapman Avenue
Orange, California 92866

Re: "Old Towne Gateway" 401 West Chapman Avenue

Dear Committee Members:

The purpose of this letter is to provide you with a more thorough response to your comments at the "Preliminary" DRC meeting held on October 20, 2020, where a face to face presentation over a scale model of the project was not possible due to COVID 19.

In recounting your comments, found in the recordings of the October 20 meeting, it is apparent that much of the proposed project is preliminarily acceptable to you with several additional design suggestions. However, the comments clearly state that the proposed new Building 4 design is unacceptable to you. Some members thought that the design was good and responsible, but that even if good, it is not appropriate to be built on this site. I believe that a comment was made something like "we like the design but that building will not be built on this site".

Our recent experience with the DRC on a much smaller project has resulted in our inability to achieve our development goals causing a reduction of scale and change in use at the host site of a small historic building. That result and the clear message we received from our committee on this project through the "Preliminary" review has caused us to re-evaluate our development program for this project.

It is clear that the design of Building 4, which we felt most appropriate for the site and the City with the interjection of a contrasting contemporary element in the historic fabric, cannot be approved by the committee. If we continue to pursue the design that satisfies our development program and which we feel most appropriate, this project would end up with a similar result as that of the recent smaller one.

Therefore we have revised the program and thus the site design to eliminate the two story mixed-use office/commercial building (Building 4) and add in its place two small brick industrial appearing buildings of similar design to that of the adjacent historic building. Our interpretation of your comments made at the October 20, "Preliminary" meeting have led us to this compromise.

This revised site design does satisfy one of the issues brought up by the Planning staff which suggested that the Building 4 infringed on the second small historic building leaving it hidden on the site. This plan celebrates the second small historic building by organizing the two new small brick buildings around the small historic building, making it a focal point in the plan.

We trust that these changes to the program and the plan will meet with your approval.

Sincerely,

LP3 architecture, Inc.

Leason F Pomeroy III, FAIA

Leason F Pomeroy III, FAIA

LETTER TO DESIGN REVIEW COMMITTEE

OLD TOWNE GATEWAY Letter of explanation/justification 08-16-21

This description of the "Old Towne Gateway" project is based on a redesign of the project following the Preliminary Design Review Committee meeting of October 21, 2020 and two (2) separate submittals to the SMART committee.

This 30,300 square foot site is built out with two contributing historic Buildings 1 and 4, and four non-contributing shed structures to be demolished including a plaster box addition to Building 1. The site is within the Depot District Plan. This proposal will bring commercial/retail uses to the site which has historically been commercial/retail and industrial.

We propose to demolish the non-contributing structures, rehabilitate the contributing historic Buildings 1 and 4 and construct a new addition to Building 1 (similar to that which is to be demolished). Build two new Buildings 2 and 3 to house commercial/retail uses, (most likely food service) and build a simple storage building to support Historic Building 1, and a shade element at the overflow outdoor dining area

The site is surrounded by commercial/industrial uses and buildings, and is in the proximity of the rail depot. The surrounding buildings are primarily tilt-up concrete, plaster, and brick which are all simple box forms with the exception of Historic Building 4 which relates in style to the nearby train depot. There are no significant negative sunlight or privacy impacts on any neighbor property posed by this project.

The design concept (image) is proposed to preserve the existing contributing historic buildings with commercial/retail uses while contrasting the addition to Historic Building 1 and its supporting storage building 5. The other new buildings, 2 and 3 shall provide an image much like that of the Historic Building 1. In an attempt to satisfy the historic guidelines and the DRC, I have proposed to utilize similar box forms in brick for the two new structures and relate directly with brick walls and truss roofs to Historic Building 1. This concept will provide a commonality in building materials and form as suggested in the Preliminary DRC meeting. The concept will also focus and bring attention to the very small Historic Building 4 with its contrasting style.

The unique characteristics of this project's location - adjacent to "Memorial Park" and the depot, being located on Chapman Avenue a main spine to the Old Towne Plan and having two contributing Historic buildings on the site, provide the opportunity to create a true gateway project to Historic Orange. And by being basically within just a few feet of the new parking structure convenient parking will be provided for customers while a "paseo" or direct path through the site will provide pedestrians a direct access from the Depot and the site to the Plaza area.

Leason F Pomeroy III, FAIA

Architect/Owner

REVISIONS

NO.	DATE
1	
2	
3	
4	
5	
6	

LP3 architecture
158 N. GLASSELL ST. STE. 201
ORANGE, CA 92866
714-771-8400

OLD TOWNE GATEWAY
401 WEST CHAPMAN AVENUE
ORANGE, CALIFORNIA 92866

SCALE:

DRAWN:

DATE:

DWG: LETTER TO DRC
LETTER OF INTRO
PROJECT INFO

SHEET NO.

C1

PROJECT INFORMATION

REQUIRED SUBMITTAL ITEMS

LETTER OF EXPLANATION

CHAPMAN AVENUE



EXISTING SITE AND DEMOLITION PLAN

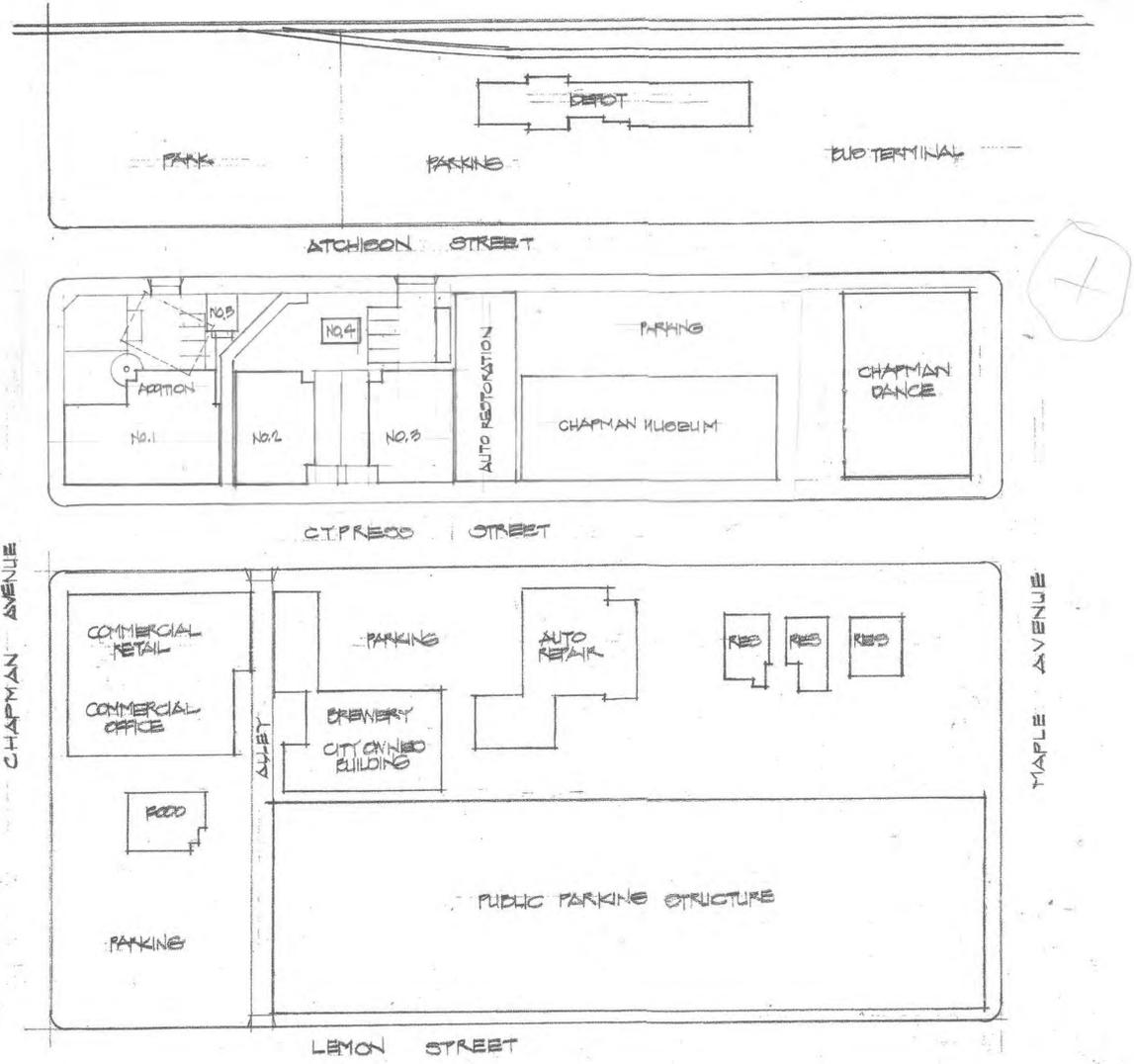
REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	

LP3 architecture
 158 N. GLASSELL ST. STE. 201
 ORANGE, CA 92866
 714-771-8400

OLD TOWNE GATEWAY
 401 WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

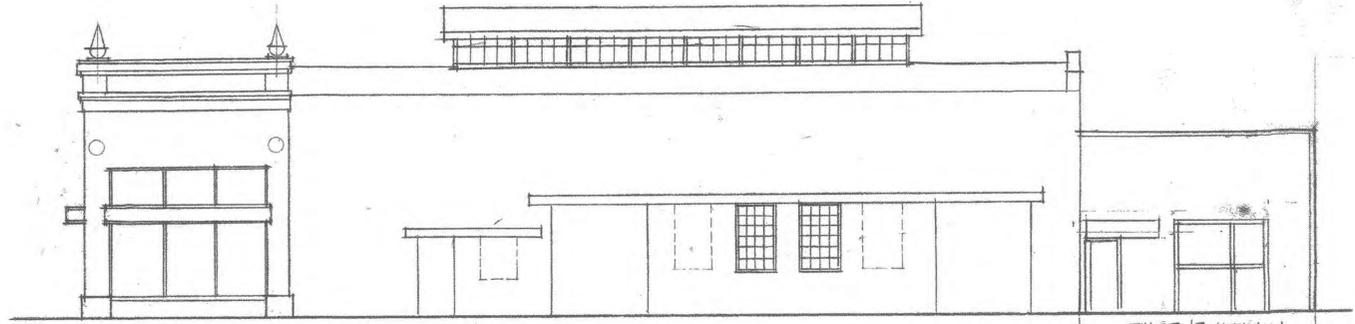
SCALE: 1/8" = 1'-0"
 DRAWN: LPP III
 DATE:
 DWG: EXISTING SITE DEMOLITION PLAN
 SHEET NO.

C2

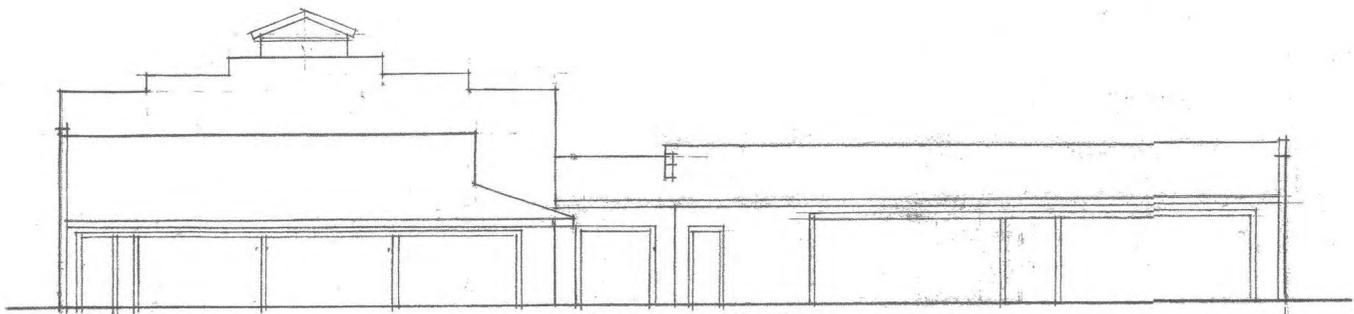


NEIGHBORHOOD SITE PLAN

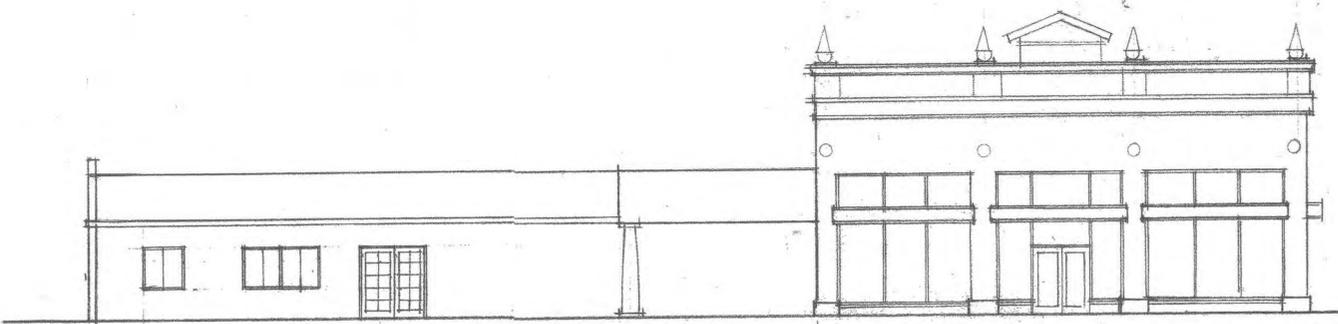
NO SCALE



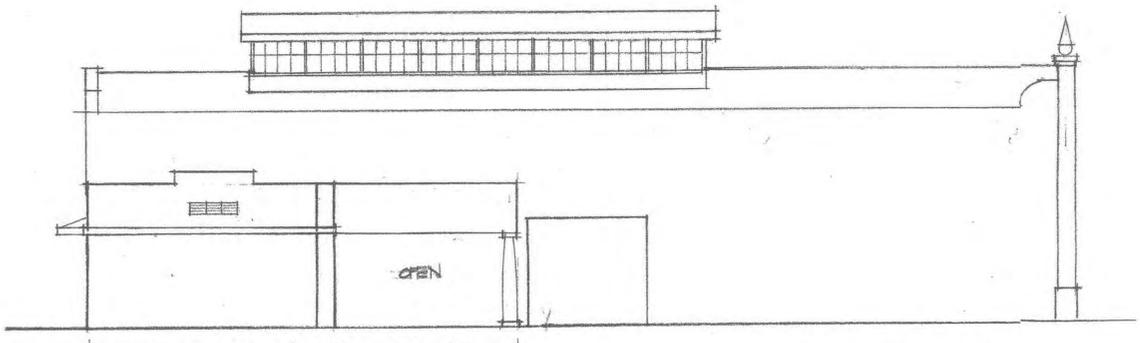
EAST ELEVATION



NORTH ELEVATION



SOUTH ELEVATION



WEST ELEVATION

ADDITIONAL ELEVATIONS - EXISTING ADDITIONS TO HISTORIC BUILDINGS I

1/8" = 1'-0"

REVISIONS	
NO.	DATE
1	
2	
3	SMART 01/26/21
4	
5	

LP3 architecture
 158 N. GLASSSELL ST. STE. 201
 ORANGE, CA 92866
 714-771-8400

OLD TOWNE GATEWAY
 401 WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

SCALE	1/8" = 1'-0"
DRAWN	LF II
DATE	
NEIGHBORHOOD PLAN EXISTING BLDG 1 ELEVATIONS	
SHEET NO.	C3

Table B - Preliminary Conditions of Approval

1.	The project shall conform in substance and be maintained in general conformance with plans and exhibits labeled including any modifications required by conditions of approval, and as approved by the City Council. Any future expansion in size or in the nature and operation of the use approved by Major Site Plan Review No. 1013-20 and Design Review No. 1011-20 shall require an application for a new or amended Site Plan Review.
2.	Except as otherwise provided herein, this project is approved as a precise plan. After any application has been approved, if changes are proposed regarding the location or alteration of any use or structure, a changed plan may be submitted to the Community Development Director for approval. If the Community Development Director determines that the proposed change complies with the provisions and the spirit and intent of the approval action, and that the action would have been the same for the changed plan as for the approved plan, the Community Development Director may approve the changed plan without requiring a new public hearing. Should the modifications be considered substantial, the modifications shall be reviewed and approved by the Planning Commission and/or Design Review Committee.
3.	These conditions shall be repeated on the second page of the construction documents when submitted to the Building Division for the plan check process.
4.	The applicant agrees to indemnify, hold harmless, and defend the City, its officers, agents and employees from any and all liability or claims that may be brought against the City arising out of its approval of this permit, and except that caused by the City's active negligence. The City shall promptly notify the applicant of any such claim, action, or proceedings and shall cooperate fully in the defense.
5.	Major Site Plan Review No. 1013-20 and Design Review No. 1011-20 shall become void if not vested within two years from the date of approval. Time extensions may be granted for up to one year, pursuant to OMC Section 17.08.060.
6.	Building permits shall be obtained for all construction work, as required by the City of Orange, Community Development Department's Building Division. Failure to obtain the required building permits may cause revocation of this entitlement.
7.	Prior to issuance of building permits for the project, the applicant shall pay all applicable development fees, including but not limited to: City sewer connection, Orange County Sanitation District Connection Fee, Transportation System Improvement Program, Fire Facility, Police Facility, Park Acquisition, Sanitation District, and School District, as required.
8.	The project approval includes certain fees and/or other excisions. Pursuant to Government Code Section 66020, these conditions constitute written notice of the fees and/or excisions. The applicant is hereby notified that the ninety (90) day protest period commencing from the date of approval of the project has begun. If the applicant fails to file a protest regarding these conditions or requirements, the applicant is legally bound from later challenging such excisions per Government Code Section 66020.
9.	In conjunction with the operation of the project, the property owner shall be responsible for maintaining the property to a level deemed adequate by the Community Development Director or designer. This includes, but is not limited to, the buildings, landscaping, recreational facilities, trash areas, signage, utilities, walls, fences, gates, and parking areas.
10.	To be determined.
11.	Prior to issuance of a Certificate of Occupancy, all landscaping improvements shall be completed according to the approved plans and to the satisfaction of the Community Development Director and the Community Services Director.
12.	All landscape areas shall be maintained in neat and healthy condition. Should any plant material die, the property owner/operator shall replace it with new healthy plant material to match the approved landscape plans.
13.	Prior to building permit issuance, final landscaping plans for the project shall be designed to comply with the City's Water Efficient Landscape Guidelines as described in Section 22.47 of the City of Orange Landmarks Standards and Specifications.
14.	Prior to building permit issuance, City required irrigation and landscape inspection notes shall be placed on the final landscape plans to the satisfaction of the Community Services Director.
15.	Prior to building permit issuance, final landscape plans for the project shall include landscape area calculations required for State-mandated landscape water use reporting.
16.	Plans submitted for Building Plan Review shall comply with the California Fire Code as amended by the City and as frequently amended and in effect at the time of application for the Building Permit.
17.	To be determined.
18.	To be determined.
19.	The final Parcel Map shall be filed and recorded prior to issuance of a building permit for the development.
20.	All works within public right-of-way and public utility easements require Encroachment Permits, including sidewalk and driveway constructions and utility work and lateral constructions.
21.	All public infrastructures, including street sections, sidewalk, driveway apron, and utilities shall comply with City of Orange Standard Plans and Specifications.
22.	New utilities serving the development, such as electric, cable television, street lighting and communication shall be installed underground, completed and approved by the appropriate utility provider prior to issuance of a Certificate of Occupancy.
23.	The corner corner ramp at the northeast corner of W. Chapman Avenue and Anaheim Street shall be constructed in conformance with Public Works Engineering Standard Plan No. 121 prior to issuance of a Certificate of Occupancy.
24.	Unused driveway approaches shall be removed and restored to full height curb and gutter, including any sidewalk restoration at the driveway apron prior to issuance of a Certificate of Occupancy.
25.	New driveway aprons shall be constructed in conformance with Public Works Standard Plan No. 115 and 116 for commercial driveway with ADA accessibility prior to issuance of a Certificate of Occupancy.
26.	Any cracked, uneven, or damaged public sidewalk, curb, and gutter along the property frontage shall be repaired prior to issuance of a Certificate of Occupancy.
27.	Sanitary sewer system connecting from the buildings to public mainline shall be private and maintained by the property owner.
28.	The applicant shall submit a grading plan in compliance with City standards for review and approval by the Public Works Director. All grading and improvements on the subject property shall be made in accordance with the Manual of Grading and Standard Plans and Specifications to the satisfaction of the Public Works Director. The applicant may be required to include Planned Erosion and Sediment Control Plans, Site Demolition Plan, and Utility Plan as part of the grading plan.
29.	The contractor shall obtain a Grading Permit from Public Works Department prior to start of any site demolition, clearing and grubbing, and grading.
30.	Any soil imported or exported shall require a Transportation Permit from Public Works Traffic Division. An approved by the City Council is required prior to import or export of soil exceeding 50,000 cubic yards. A dirt haul permit shall be approved by the City Council and the permit fee is subjected to pavement deterioration evaluation per Orange Municipal Code 10.67.030(B).
31.	Upon submittal of grading plan for plan check, the applicant shall pay a deposit to cover plan check and inspection services related to the grading activities as specified in the City's Master Schedule of Fees.
32.	The grading plan shall include the following: <ul style="list-style-type: none"> • Detail of all of the locations where retaining walls will be constructed. Geometric detail of retaining walls shall be shown on the grading plan, including material type, dimensions, height, and subdrains. A building permit is also required for retaining walls over 4 feet in height measured from the bottom of the footing to the top of the wall prior to construction. Structural details and design calculations shall be submitted as a separate document and will be reviewed and permitted by City Building Division. • Geotechnical report • Trash receptacle locations and details. The trash storage area shall be constructed per Public Works Standard Plan 409. • All sewer and storm drain lines. Other utility lines, such as water lines, may also be shown on Grading Plan for reference. • All sewer and storm drain lines. Other utility lines, such as water lines, may also be shown on Grading Plan for reference. • All structural Best Management Practices (BMPs) for water quality purposes. Water quality features shown on the Grading Plan must match Water Quality Management Plan.
33.	Any grading outside of the owner's property boundary shall require the applicant to either obtain a temporary construction easement or permission by adjacent property owners in a form acceptable to the Public Works Director.
34.	The property owner shall maintain in good condition all on-site driveways where heavy-duty trucks would travel.
35.	Prior to building foundation construction, a Certificate of Line and Grade shall be submitted to Public Works Construction Inspector demonstrating that the site grading and plan elevations are completed according to the grading plan.
36.	Prior to issuance of building permits, the applicant shall submit an address number request, including an addressing plan, to Public Works Department for review and approval.
37.	The address number of each commercial building shall be illuminated during the hours of darkness so that it shall be easily visible from the street. The materials in these numbers shall be no less than six inches in height and be of a color contrasting to the background. In addition, any business which affords vehicular access to the rear through any driveway, alleyway or parking lot shall display the same numbers on the rear of the building.
38.	To be determined.
39.	Prior to building permit issuance, the applicant shall submit improvement plans to the Water Division for review and approval for any new fire hydrants, domestic water services, fire services, landscape services, and any other proposed improvements or relocations affecting the public water system facilities.
40.	Prior to issuance of the certificate of occupancy, the applicant shall be responsible for the installation of necessary fire hydrants and fire services as approved by the Fire Department and Water Division.
41.	Prior to building permit issuance, the Water Division shall approve the type and location of landscaping and fire service (backflow prevention) device for proposed City services.
42.	Prior to building permit issuance, construction documents shall show that a six-foot minimum horizontal clearance and a one-foot minimum vertical clearance would be maintained between City water mains, laterals, services, meters, fire hydrants and all other utilities except sewer. The Water Division shall review and approve the construction documents.
43.	Prior to building permit issuance, construction documents shall show that an eight-foot minimum clearance is provided between City water mains, laterals, services, meters, fire hydrants, signs, or trees or other substantial obstructions and plans as required by the Water Division. The Water Division shall review and approve the construction documents.
44.	Prior to building permit issuance, construction documents shall show that permanent signs, marking, method water quality management features or other structures are not built over water mains, laterals, services, meters, or fire hydrants as required by the Water Division.
45.	Prior to building permit issuance for the first phase of work, the applicant shall be responsible for obtaining approval of all of the necessary encroachment permits from affected agencies for all public water construction work.
46.	Prior to approval of a water improvement plan, the applicant shall satisfy all water main connection, plan check, and inspection charges as determined by the Water Division.
47.	Prior to the issuance of any grading permit, the applicant shall construct all public and/or private improvements to the satisfaction of the Water Division. The applicant may be required to enter into an agreement with the City of Orange, and post security in a form and amount acceptable to the City Engineer and/or Water Division to ensure construction of said improvements.
48.	Plans submitted during plan check shall show that the water improvement plans are consistent with the fire suppression plans and or fire master plan. The applicant's consultant preparing the water improvement plans shall coordinate their plans with the consultant preparing the fire suppression plans and/or fire master plan so that their designs conform.

49.	Plans submitted during plan check shall show that the minimum separation requirements are met and that each of the various designer's plan sets match. The applicant's consultant preparing the improvement and utility plans shall coordinate their plans with the consultants preparing the landscape, architectural, surface water quality management, fire master and/or fire suppression plans so that their designs are consistent.
50.	At least fourteen calendar days prior to commencing construction, the applicant's civil engineer shall prepare and provide product material submittals consistent with the water improvement plans for all proposed public water system facilities to the Water Division per the City of Orange General Water Construction Notes for review and approval.
51.	Prior to issuance of certificate of occupancy, the applicant shall furnish and install individual pressure regulators on new services where the incoming pressure exceeds eighty-pounds per square inch.
52.	Prior to the issuance of any grading permits the applicant shall submit a Priority Project Water Quality Management Plan (WQMP) for review and approval to the Public Works Department: <ul style="list-style-type: none"> a) Prioritize the use of Low Impact Development principles as follows: preserve natural features; minimize runoff and reduce impervious surfaces; and utilize infiltration of runoff as the method of pollutant treatment. Infiltration BMPs to be considered include the use of permeable materials such as concrete and concrete pavers, infiltration trenches, infiltration basins, and other infiltration BMPs as applicable. b) Incorporate the applicable Site Design, Routine Source, Structural Control and Low Impact BMPs as defined in the Model Water Quality Management Plan and Technical Guidance Document. c) Maintaining the hydrologic characteristics of the site by matching time of concentration, runoff, velocity, volume and hydrograph for a 2-year event. d) Minimize the potential increase in downstream erosion and avoid downstream impacts to physical structures, aquatic and riparian habitat. e) Generally describe the long-term operation and maintenance requirements for structural and Treatment Control BMPs. f) Identify the entity or employee that will be responsible for long-term operation, maintenance, repair and or replacement of the structural and Treatment Control BMPs and the training that qualifies them to operate and maintain the BMPs. g) Describe the mechanism for funding the long-term operation and maintenance of all structural and Treatment Control BMPs. h) Include a copy of the forms to be used in conducting maintenance and inspection activities. i) Meet recordkeeping requirements (forms to be kept for 5 years). j) Include a copy of the forms to be submitted annually by the project owner to the Public Works Department that certifies that the project's structural and treatment BMPs are being inspected and maintained in accordance with the project's WQMP.
53.	Prior to the issuance of certificate for use of occupancy, the applicant shall demonstrate the following to the Public Works Department: <ul style="list-style-type: none"> a) That all structural and treatment control best management practices (BMP) described in the Project WQMP have been constructed and installed in conformance with the approved plans and specifications. b) That the applicant is prepared to implement all non-structural BMPs described in the Project WQMP. c) That an adequate number of copies of the project's approved final project WQMP are available for the future occupiers.
54.	Prior to the issuance of certificate for use of occupancy or final sign-off by the Public Works Department, the applicant shall demonstrate to the satisfaction of Public Works, that the project's WQMP has been reviewed by the BMP maintenance requirements in Section 7 of the WQMP with the responsible person and that a copy of the WQMP has been provided to that person. A certification letter from the WQMP preparer may be used to satisfy this condition.
55.	Prior to issuance of building permits, the applicant shall review the approved Water Quality Management Plan (WQMP) and grading plan to ensure the structure's downspouts or drainage outlet locations are consistent with those documents. Copies of the building or architectural plans specifically showing the downspouts and drainage outlets shall be submitted to the Public Works Department for review.
56.	The project applicant shall maintain all structural, treatment and low impact development BMPs at the frequency specified in the approved WQMP. Upon transfer of ownership or management responsibilities for the project site, the applicant shall notify the City of Orange Public Works Department of the new person(s) or entity responsible for maintenance of the BMPs.
57.	Prior to City approval of the landscape plans, the applicant shall review the approved Water Quality Management Plan and ensure the proposed landscape plans are consistent with the project grading plans. The plans must show any proposed storm water treatment Best Management Practices (BMPs) such as stormwater planters, drywells, permeable pavers, or any other proposed surface water quality BMPs.

Building Conditions and Design Criteria

CITY OF ORANGE
 Building Division
 300 E Chapman Avenue, Orange CA 92668
 Office: 714-744-7200 Fax: 714-744-7245
 Website: www.cityoforange.com

The City of Orange has adopted the series of Codes:

- > 2019 California Building Code (CBC), which is based on the 2018 International Building Code (IBC) published by the International Code Council (ICC).
- > 2019 California Residential Code (CRC), which is based on the 2018 International Residential Code (IRC) published by the International Code Council (ICC).
- > 2019 California Electrical Code (CEC), which is based on the 2017 National Electrical Code (NEC) published by the National Fire Protection Association (NFPA).
- > 2019 California Mechanical Code (CMC), which is based on the 2018 Uniform Mechanical Code (UMC) published by the International Association of Plumbing and Mechanical Officials (IAPMO).
- > 2019 California Plumbing Code (CPC), which is based on the 2018 Uniform Plumbing Code (UPC) published by the International Association of Plumbing and Mechanical Officials (IAPMO).
- > 2019 California Energy Code (CEC), which is written by the California Energy Commission (CEC), and published by the California Building Standards Commission.
- > 2019 California Fire Code (CFC), which is based on the 2018 International Fire Code (IFC) published by the International Code Council (ICC).
- > 2019 California Green Building Standards Code (CGBCS), published by the California Building Standards Commission.

City of Orange is located in Seismic Design Category D.

The ultimate wind speed is 95 mph; this can be verified with ASCE 7-16, chapter 26, 2019 CBC Table 1609.3(1). Most areas of Orange can be classified as wind exposure category C. However, the design professional is responsible for justifying the exposure selected for the specific area of construction.

For additions and remodels, the soil bearing pressure is based on 1,500 pounds per square foot, unless a higher value is justified by a soils report. A 1/3 increase is allowed for wind/seismic.

For retaining walls, lateral soil load is based on 2019 CBC Table 1610.1. And lateral soil bearing pressure is based on 100 pounds per square foot per foot below natural grade, unless a higher value is justified by a soils report. Without a soils report the coefficient of friction is 130 psf x contact area of footing or 1/2 DL whichever is less.

The climate zone is Zone 8.

Rainfall design is based on 2 inches of rainfall per hour.

Minimum roof loads is based on 2019 CBC Table 1607.1.

Provide design data on the first sheet:
 a. Scope of work (Description of use)
 b. Occupancy groups
 c. Type of construction
 d. Sprinkler Building
 e. Floor Area
 f. Height
 g. Number of Stories
 h. Allowable Floor Area
 i. APN (Assessor parcel Number).

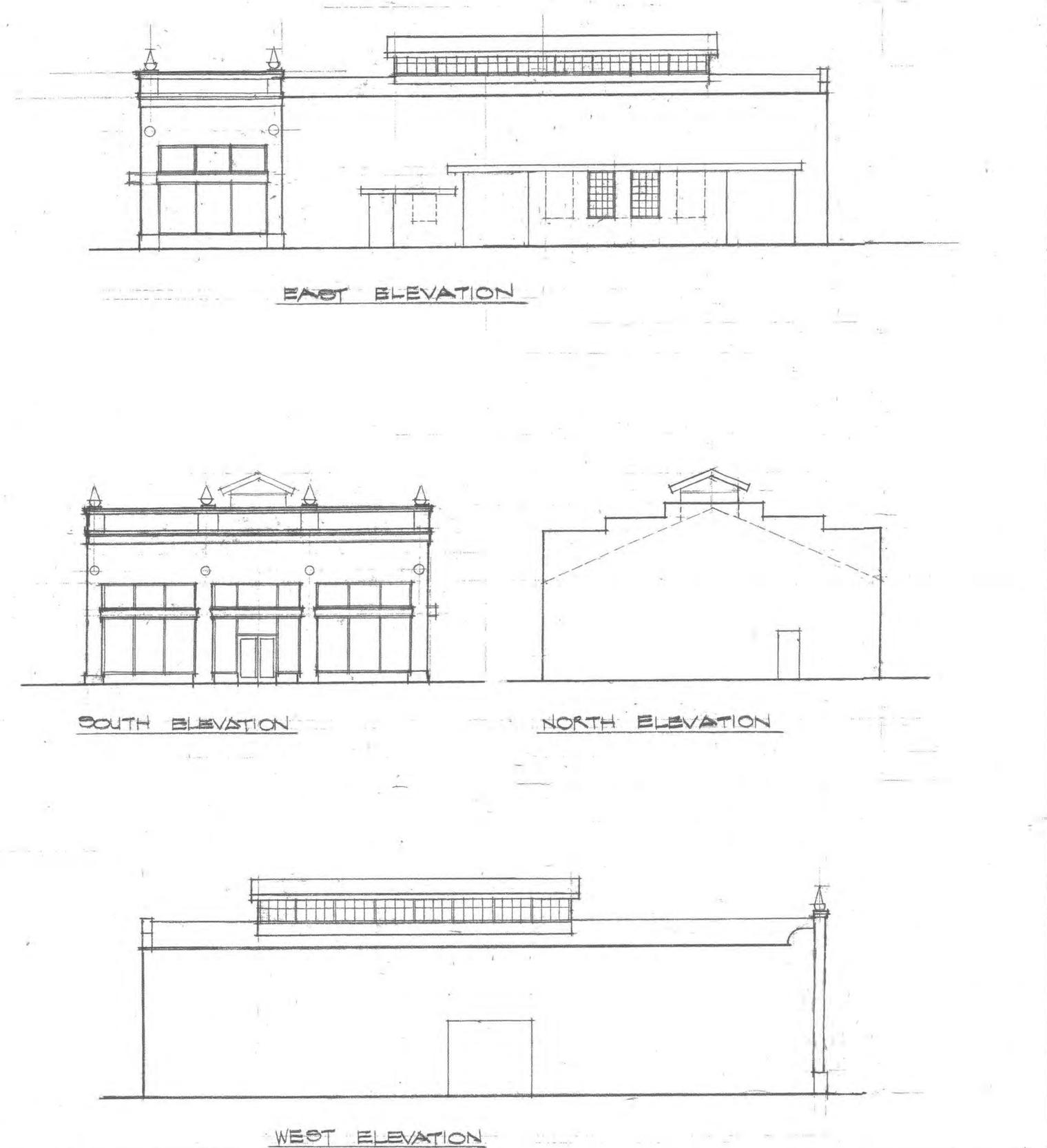
A complete plot plan showing: Lot dimension, yard setbacks, street name(s), north arrow, and easement.

DISABLED ACCESS

- a. Show (provide) the accessible new and existing parking (including van accessible parking) and reference detail of the standard accessible parking, path of travel to building and to public way, ramp and signage on the site plan.
- b. Show on the plan accessible restroom and the cross-reference of the standard accessible restroom detail.
- c. Provide accessible for cash register counter or reception desk.
- d. Provide specifications for the door hardware to comply with disabled access requirements. (Lever type, push-pull, panic, etc)
- e. Provide and detail tactile exit signage and locations.

EGRESS

- a. Submit an exit plan that labels and clearly shows compliance with all required egress features such as, but not limited to, common path of travel, required number of exits, occupant load, required width, continuity, travel distance, etc. CBC 1001.1.



REVISIONS	
NO.	DATE
1	START 01/21/20
2	
3	
4	
5	
6	

LP3 architecture
 158 N. GLASSELL ST. STE. 201
 ORANGE, CA 92866
 714-771-8400

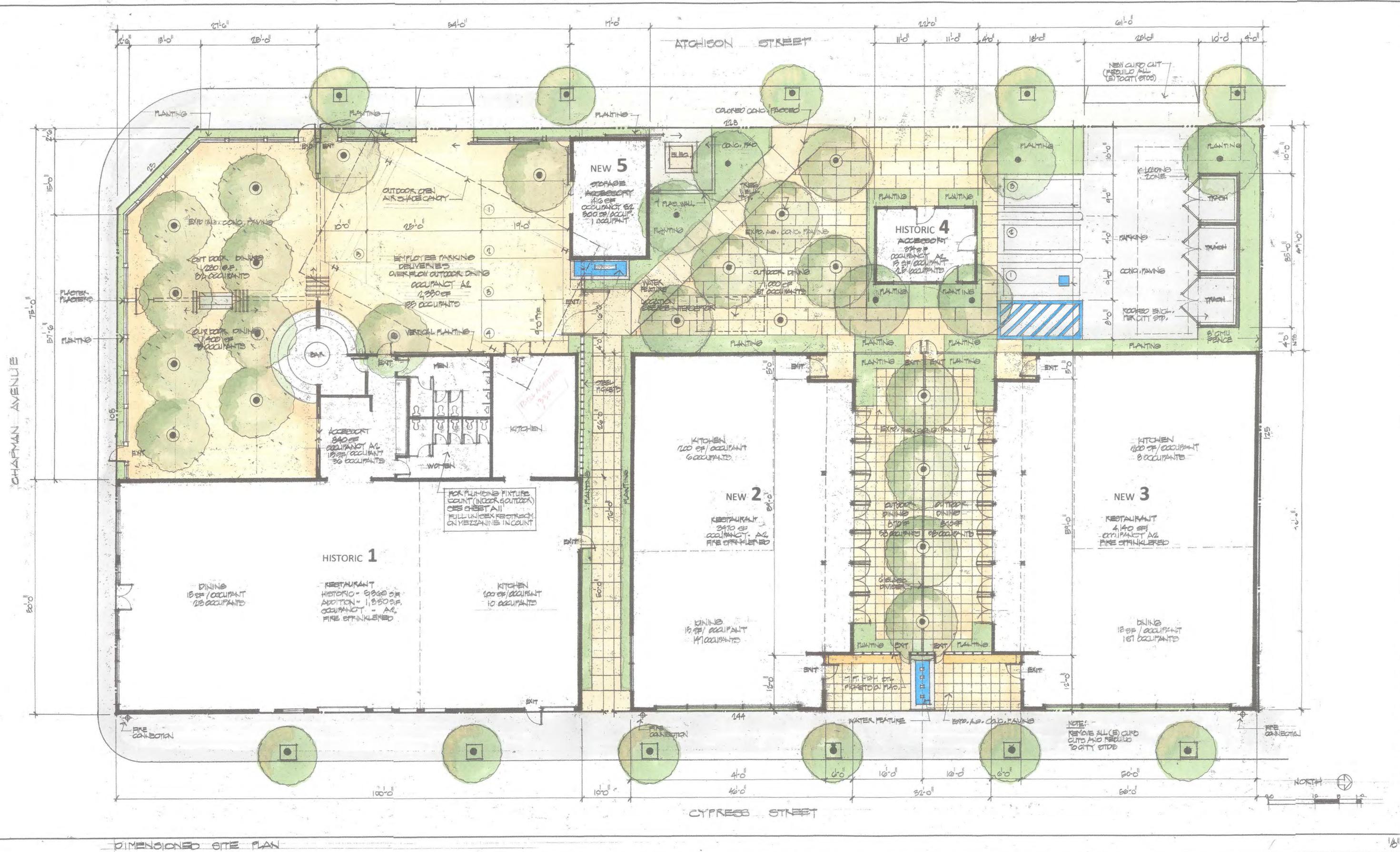
OLD TOWNE GATEWAY
 401 WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

SCALE:	1/8" = 1'-0"
DRAWN:	LP3
DATE:	
DWG:	BUILDING 1
	CITY DESIGN CRITERIA
SHEET NO.:	C4

PRELIMINARY CONDITIONS OF APPROVAL

BUILDING DESIGN CRITERIA

HISTORIC BUILDING 1 EXTERIOR ELEVATIONS



REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	

LP3 architecture
 158-N. GLASSELL ST. STE. 201
 ORANGE, CA 92866
 714-771-8400

OLD TOWNE GATEWAY
 401 WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

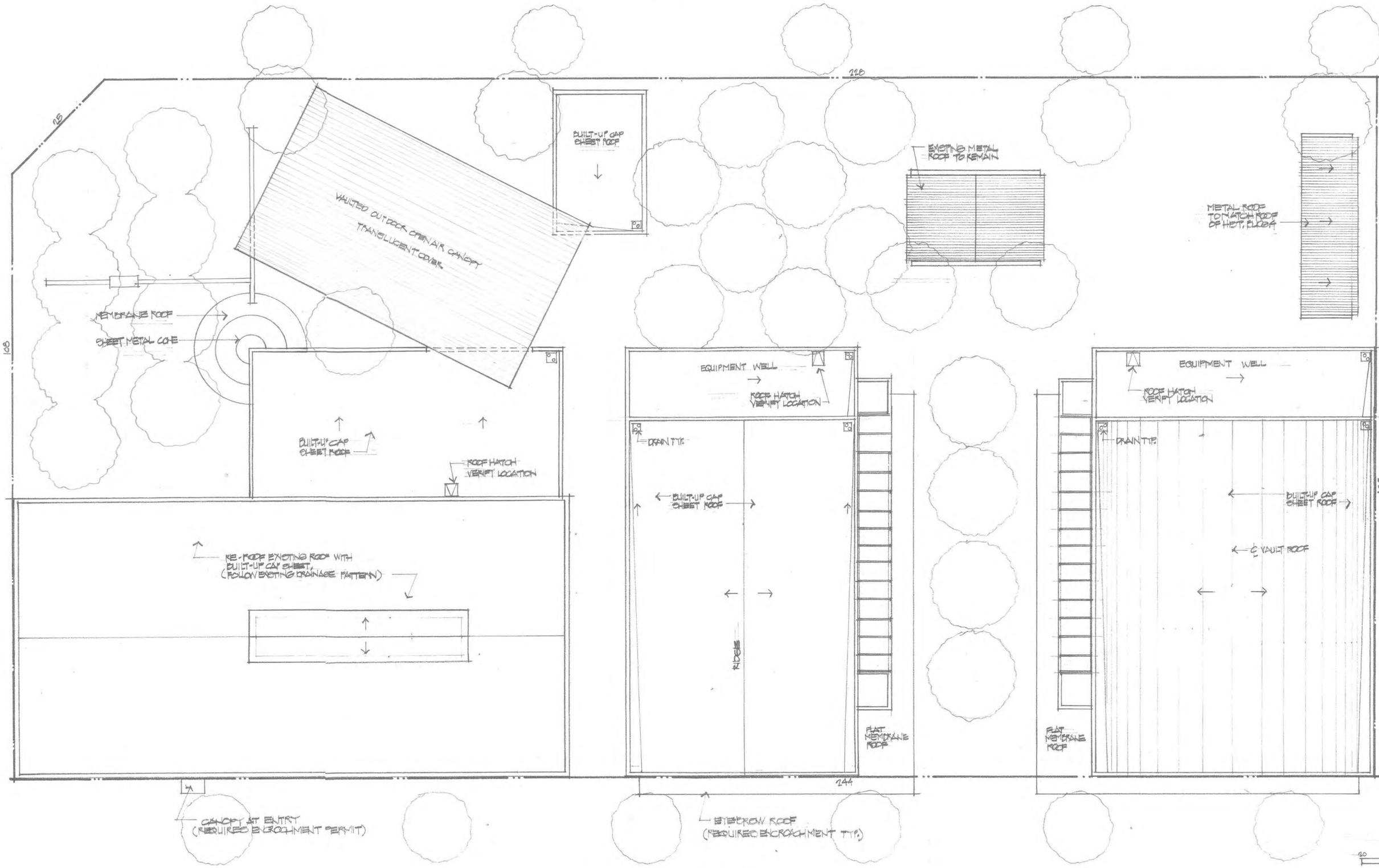
SCALE:	1/8"=1'-0"
DRAWN:	LP3 III
DATE:	
DWG:	ILLUSTRATIVE SITE PLAN
SHEET NO.	A1

DIMENSIONED SITE PLAN

CHAPMAN AVENUE

ATCHISON STREET

CYPRESS STREET

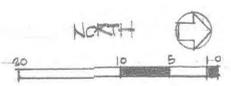


REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	

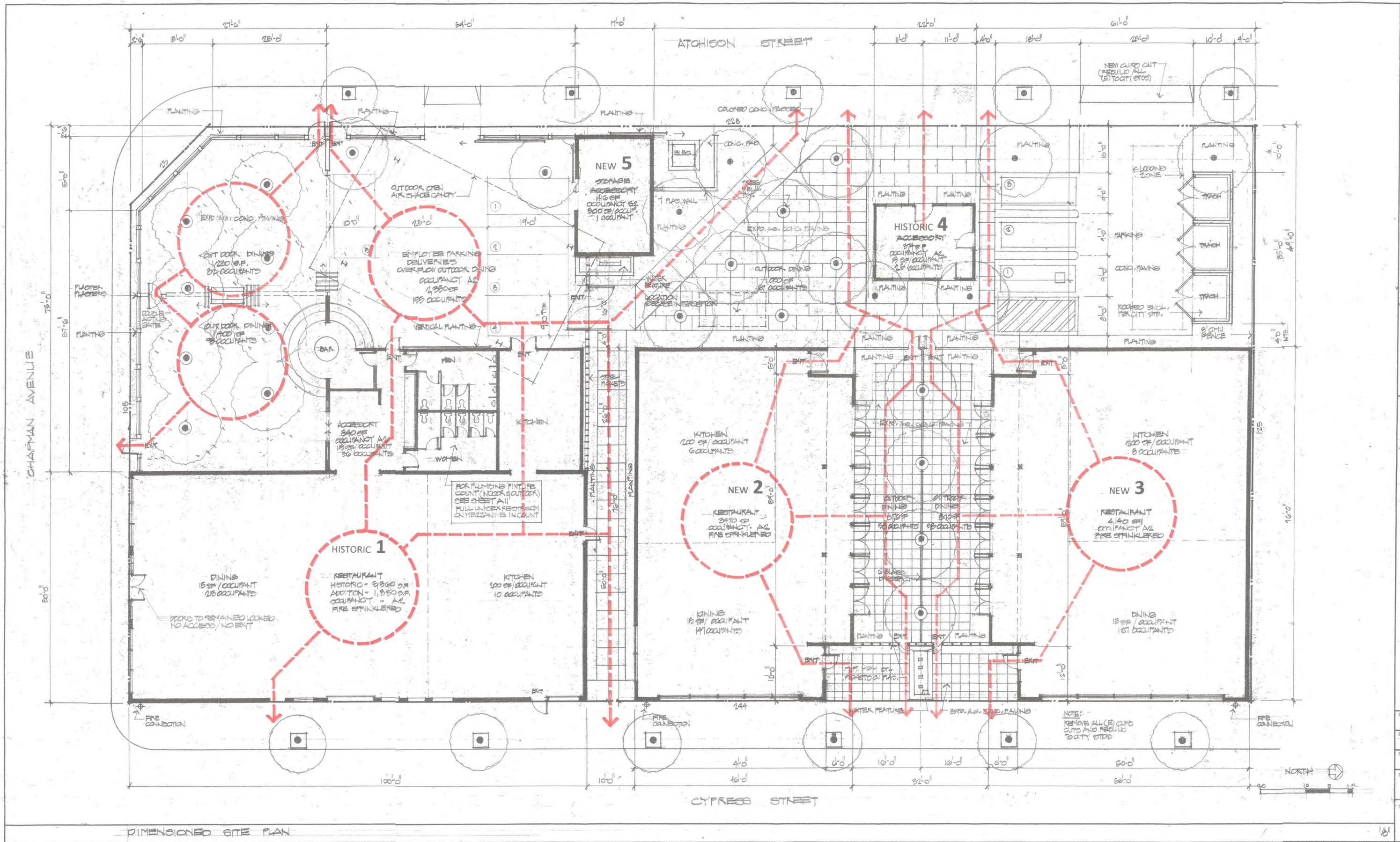
LP3 architecture
 158 N. GLASSELL ST. STE. 201
 ORANGE, CA 92866
 714-771-8400

OLD TOWNE GATEWAY
 401 WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

SCALE:	1/8" = 1'-0"
DRAWN:	HP #
DATE:	
DWG:	SITE ROOF PLAN
SHEET NO.	A3



1/8"



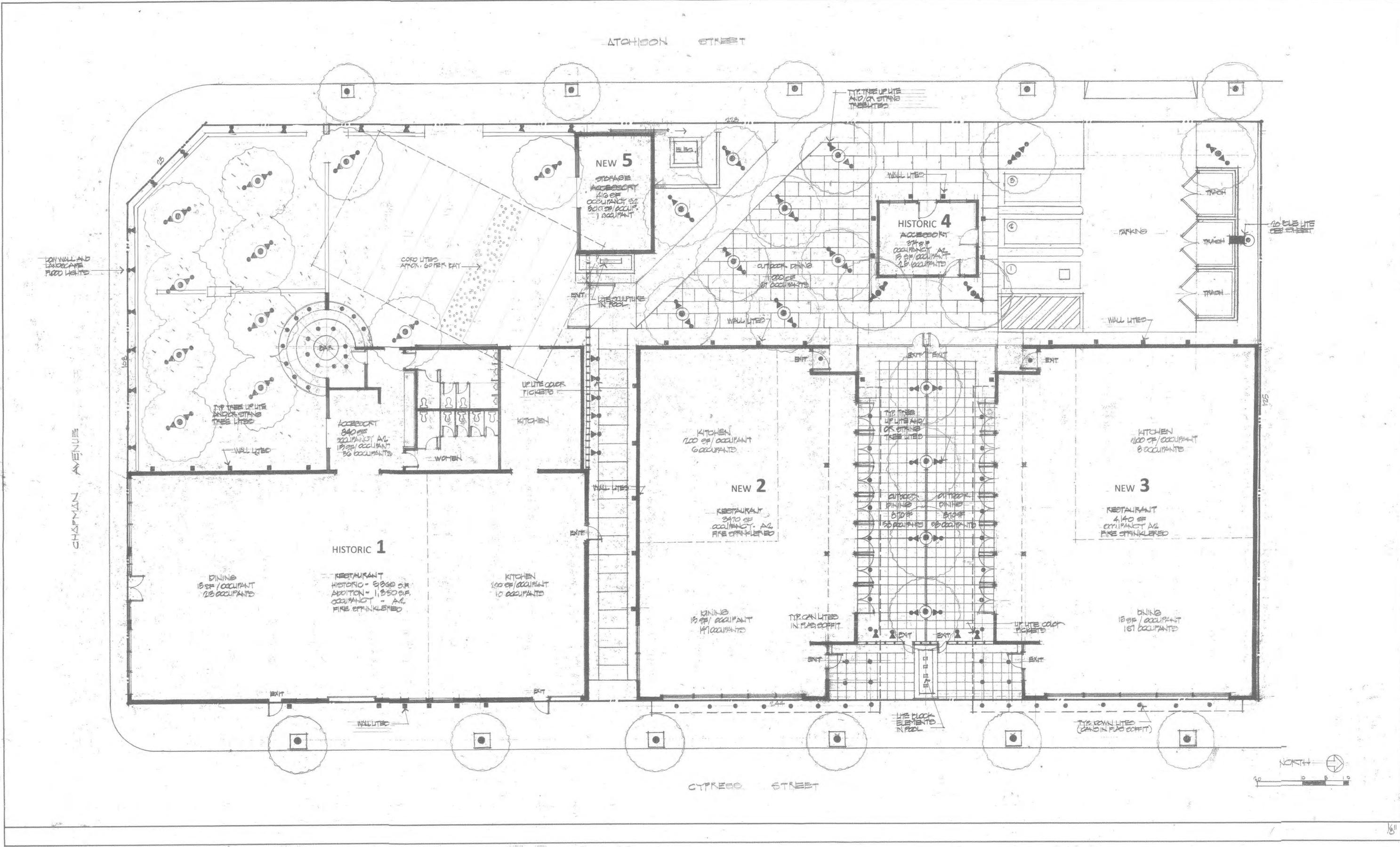
DIMENSIONED SITE PLAN

REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	

LP3 architecture
 158-N. GLASSELL ST. STE. 201
 ORANGE, CA 92866
 714-771-8400

OLD TOWNE GATEWAY
 401, WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

SCALE: 1/8"=1'-0"
DRAWN: LP3 II
DATE:
DWG: SITE EXITING PLAN
SHEET NO. A4



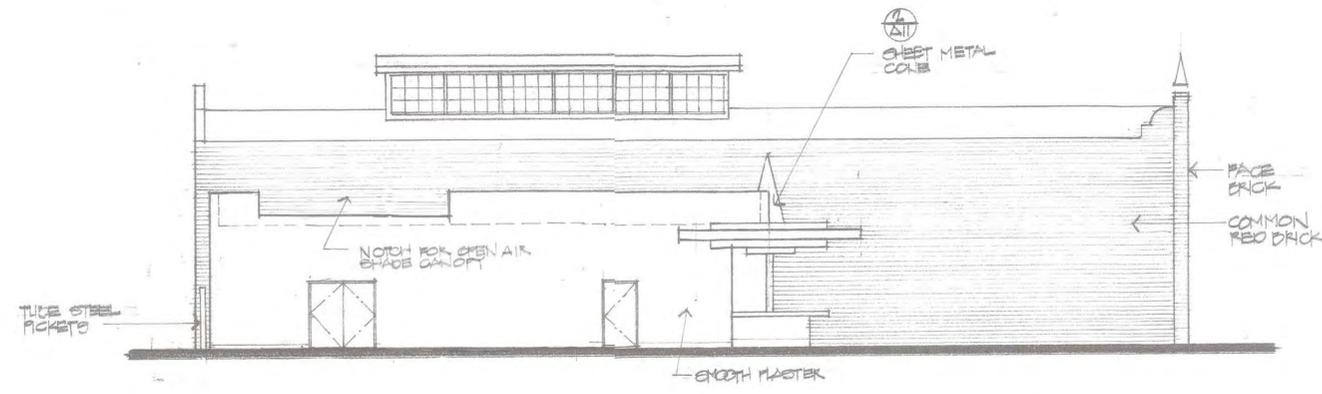
REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	

LP3 architecture
 158 N. GLASSELL ST. STE. 201
 ORANGE, CA 92866
 714-771-8400

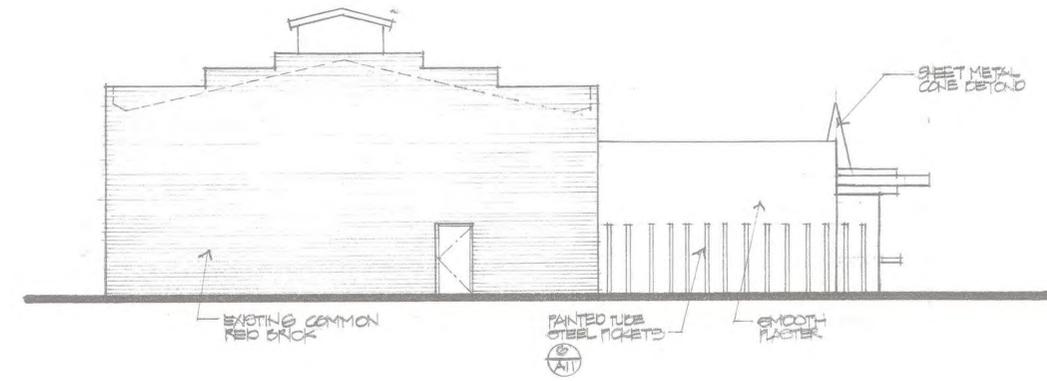
OLD TOWNE GATEWAY
 401 WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

SCALE: 1/8"=1'-0"
 DRAWN: LP3
 DATE:
 DWG: CONCEPTUAL SITE LIGHTING PLAN
 SHEET NO.

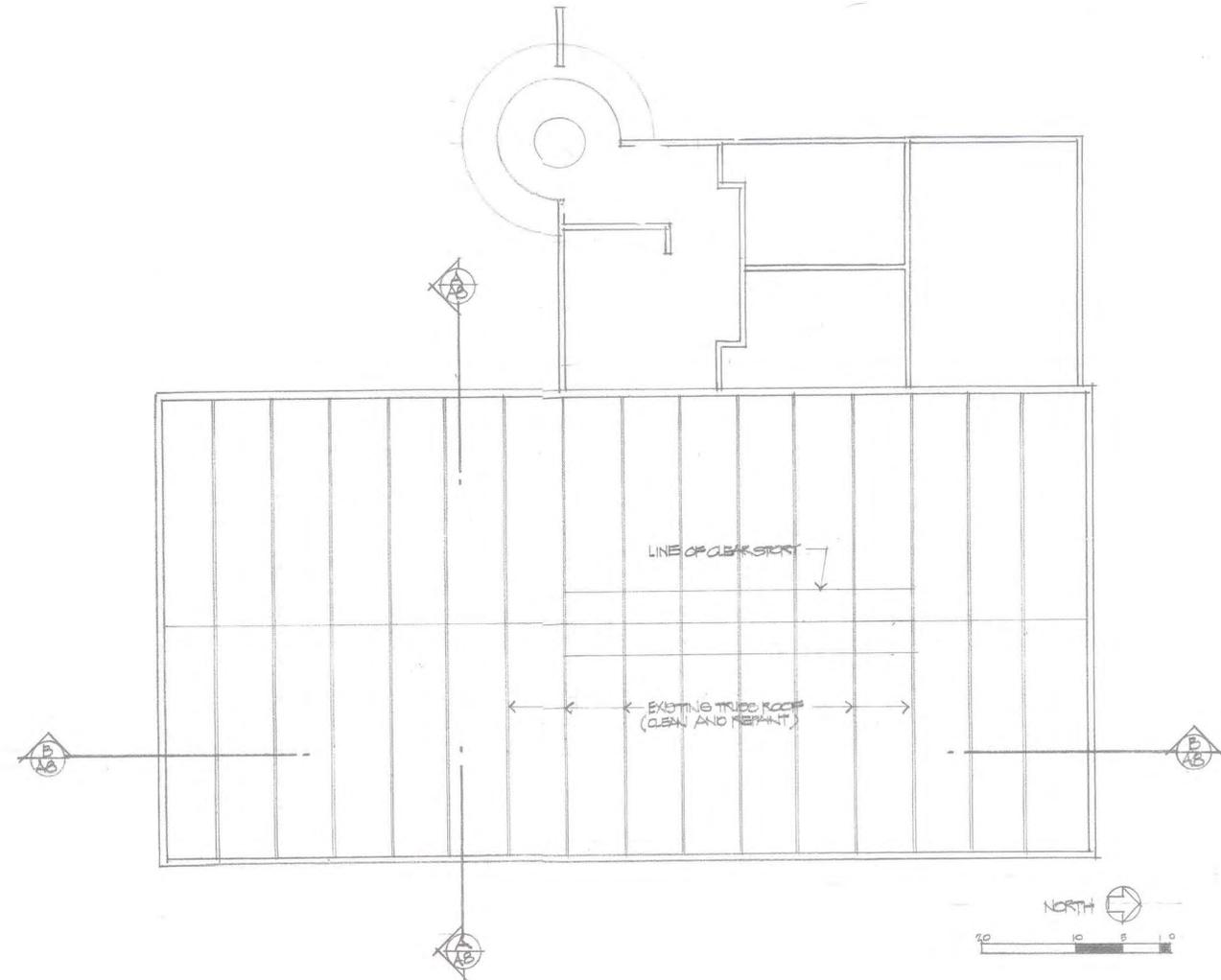
A5



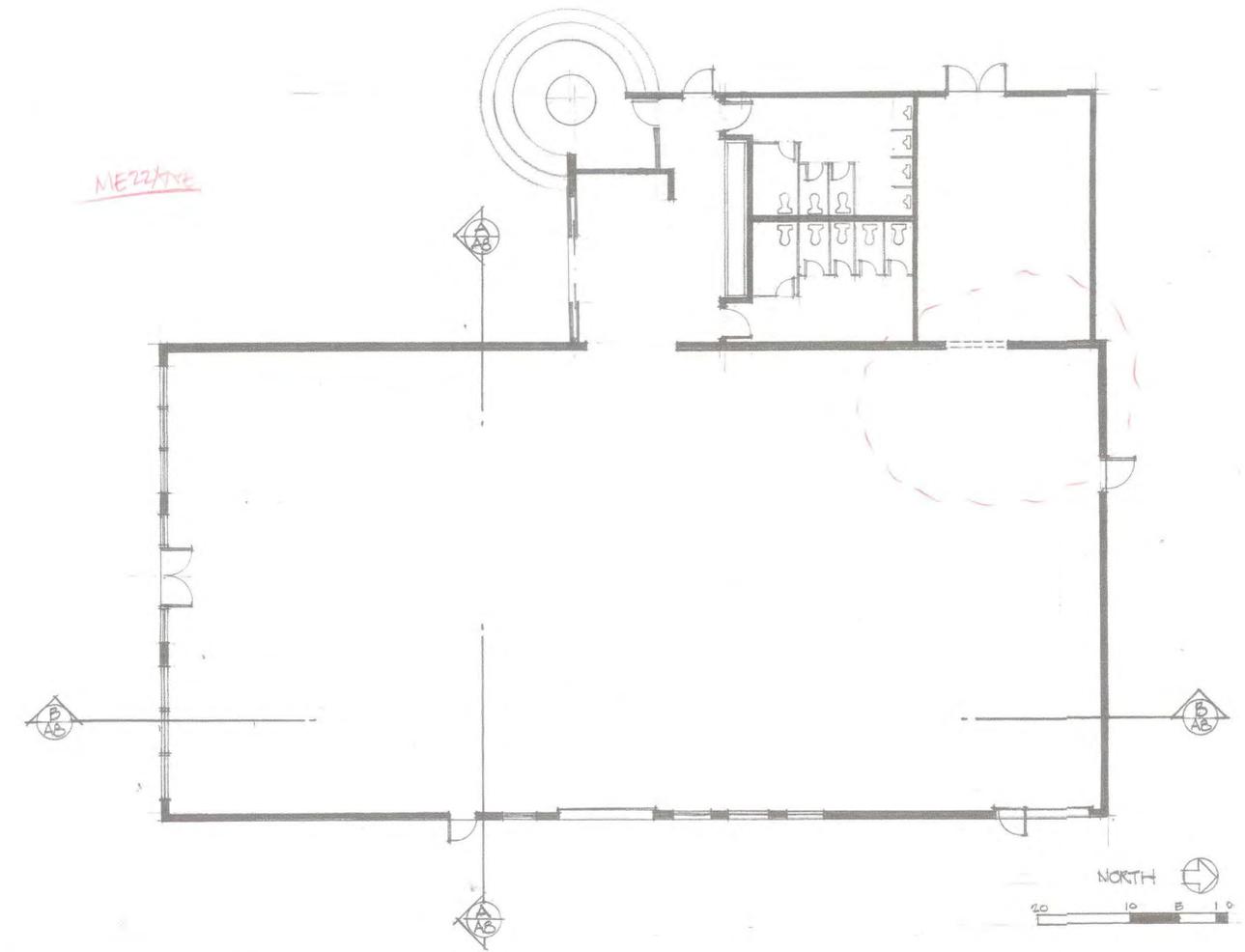
WEST ELEVATION HISTORIC BUILDING 1 WITH ADDITION



NORTH ELEVATION HISTORIC BUILDING 1 WITH ADDITION (VIEW FROM FACED)



HISTORIC BUILDING 1 AND ADDITION REFLECTED CEILING PLAN



HISTORIC BUILDING 1 AND ADDITION FLOOR PLAN

REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	

LP3 architecture
 158 N. GLASSSELL ST. STE. 201
 ORANGE, CA 92866
 714-771-8400

OLD TOWNE GATEWAY
 401 WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

SCALE: 1/8" = 1'-0"

DRAWN: LFP III

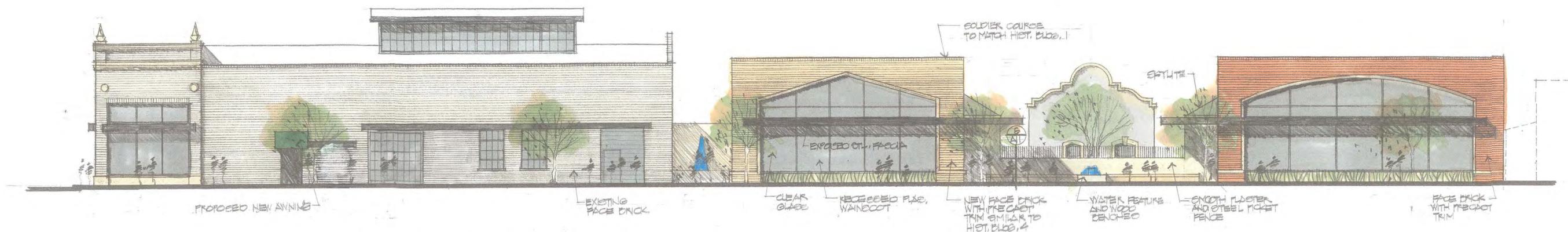
DATE:

HISTORIC BUILDING 1
 FLOOR PLAN
 ELEVATIONS

SHEET NO.

A6

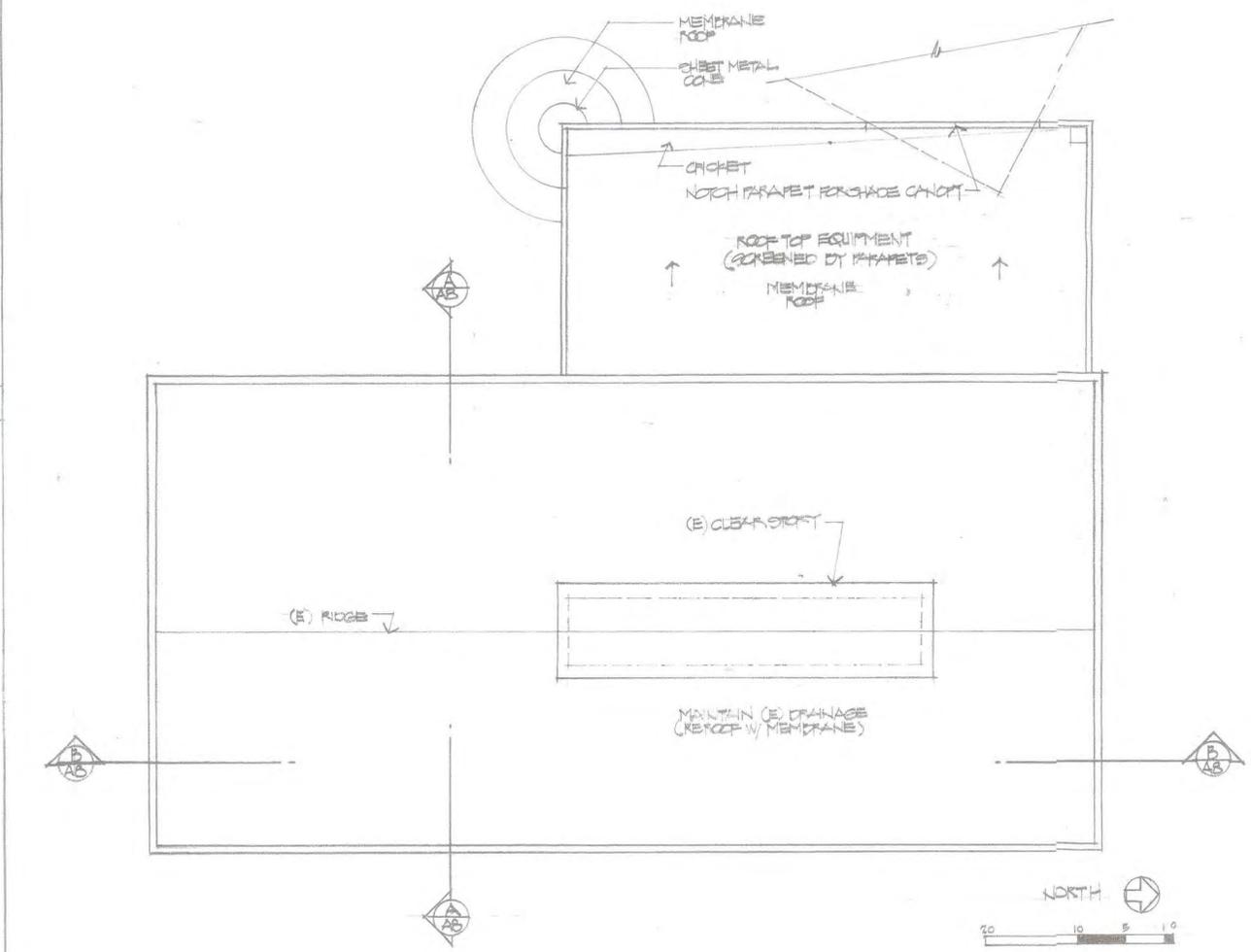
REVISIONS	
NO.	DATE



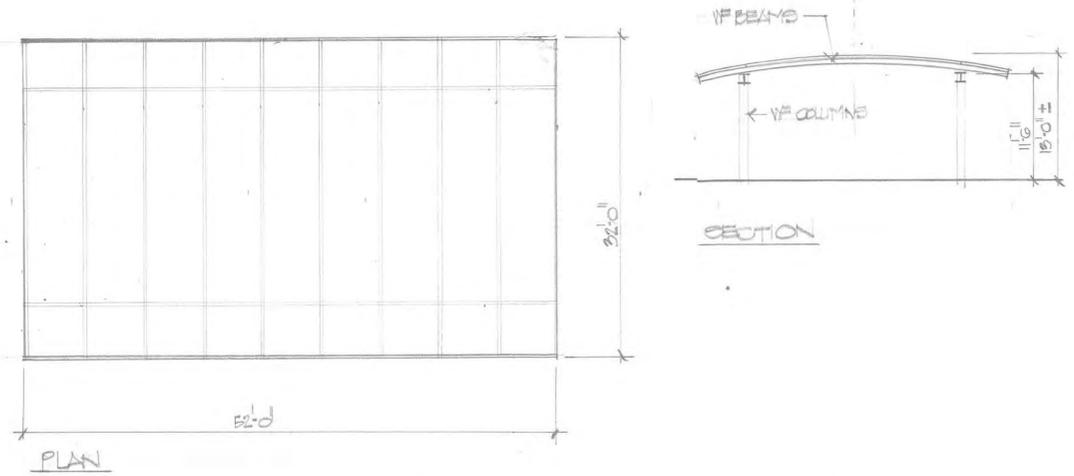
CYPRESS STREET ELEVATION



CHAPMAN AVENUE ELEVATION



BUILDING 1 ROOF PLAN

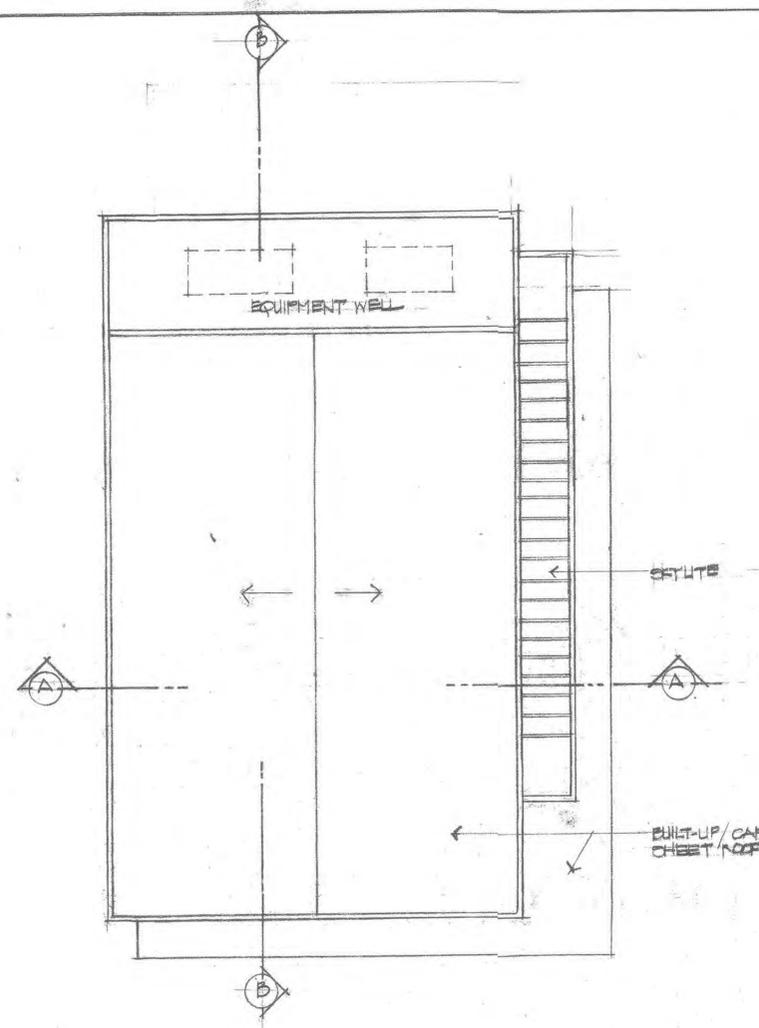


OUTDOOR CANOPY

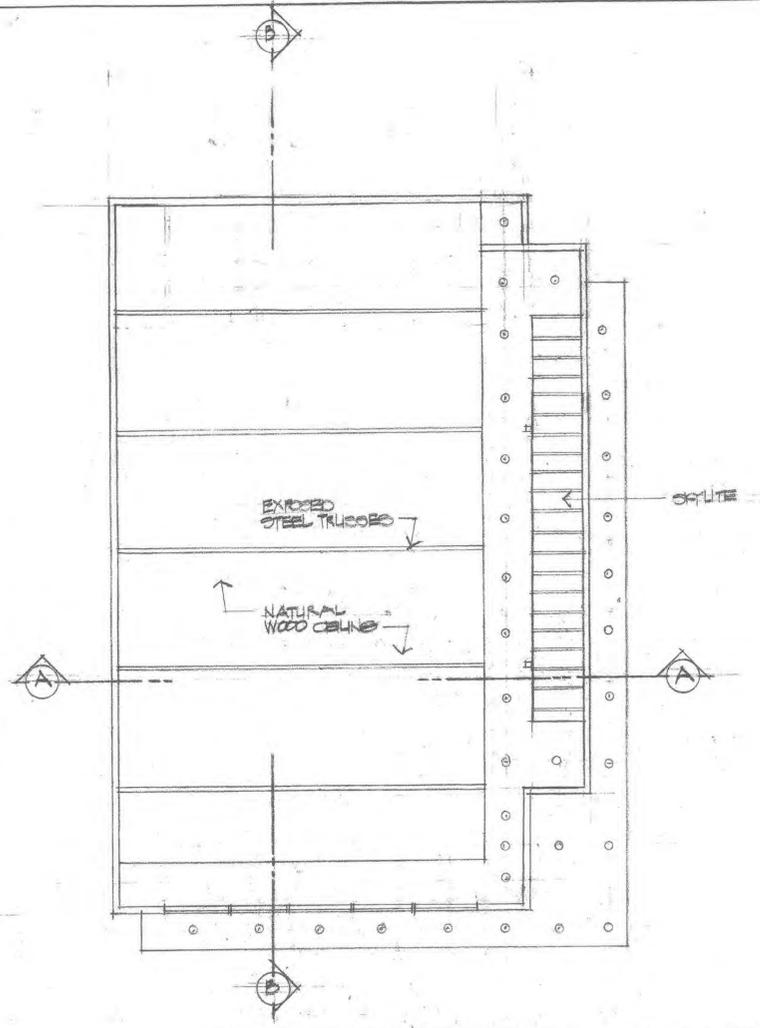
LP3 architecture
 158 N. GASSELL ST. STE. 201
 ORANGE, CA 92866
 714-771-8400

OLD TOWNE GATEWAY
 401 WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

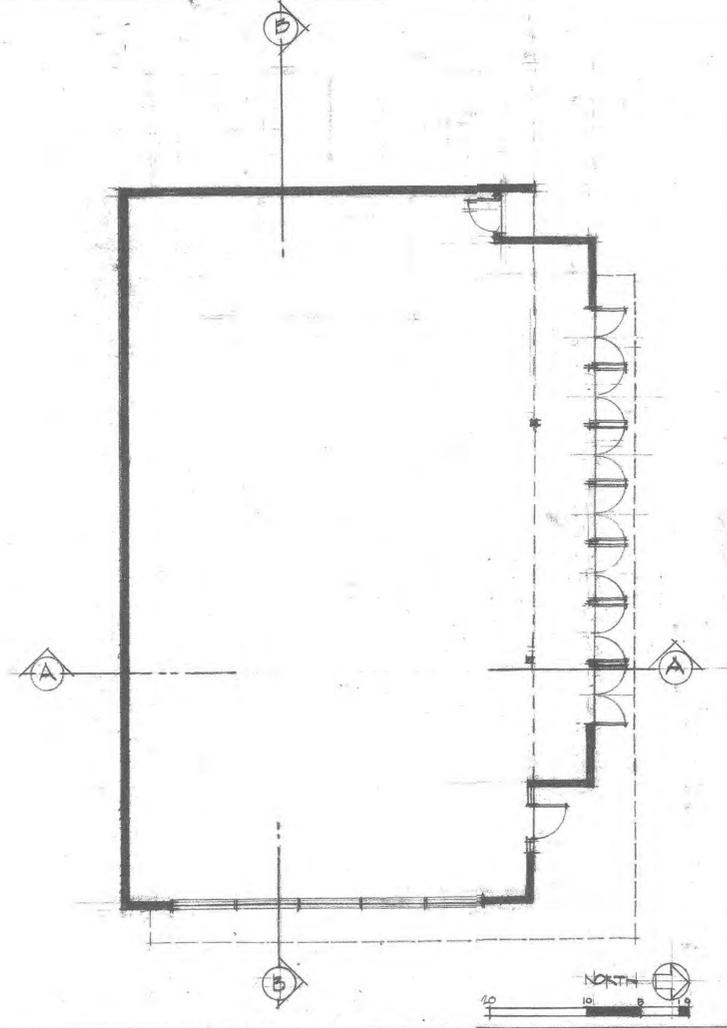
SCALE: AS NOTED
 DRAWN: LP3
 DATE:
 HISTORIC BUILDING 1
 ROOF PLAN
 STREET ELEVATIONS
 SHEET NO. **A7**



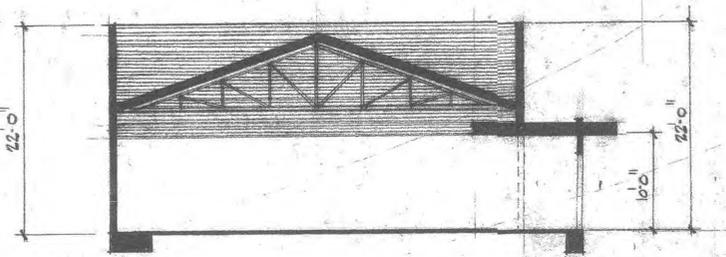
ROOF PLAN



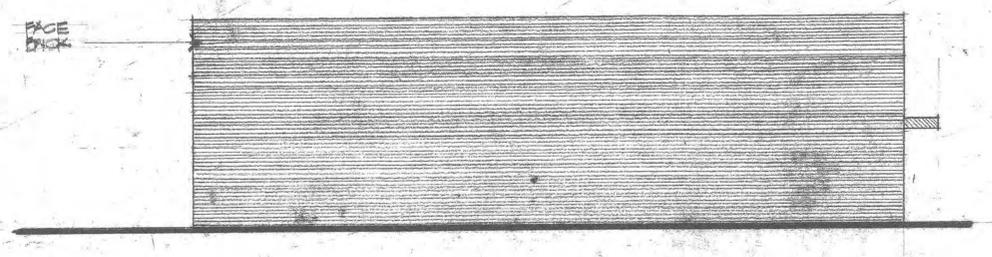
CEILING PLAN



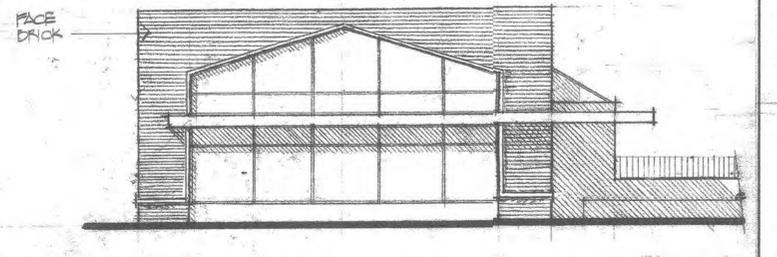
FLOOR PLAN



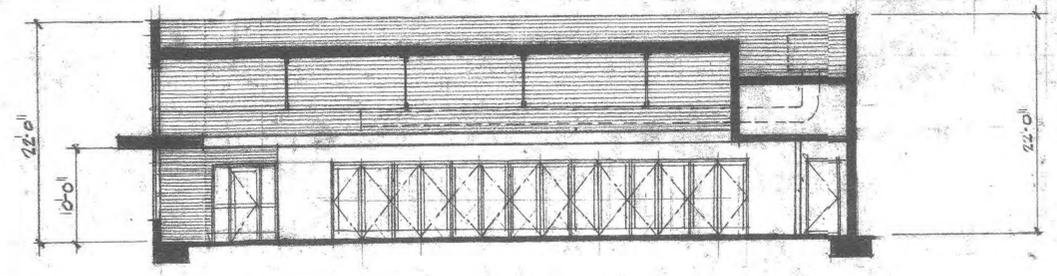
A BUILDING SECTION - LOOKING WEST



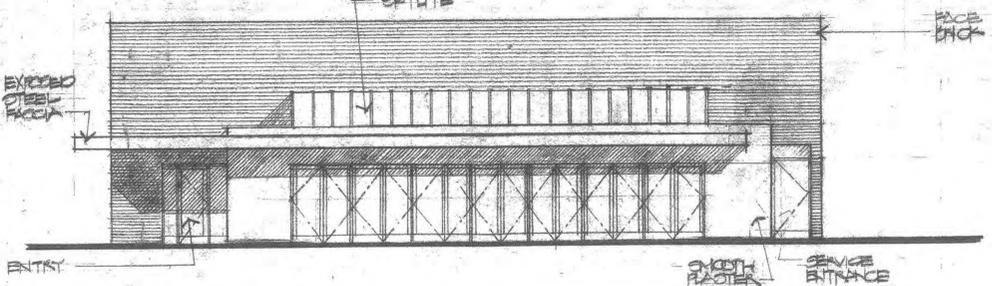
SOUTH ELEVATION



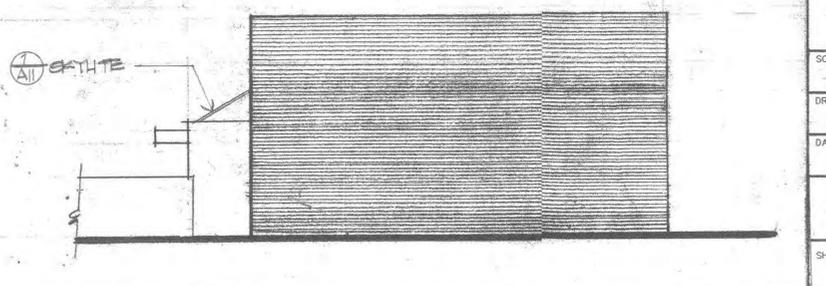
EAST ELEVATION



B BUILDING SECTION - LOOKING NORTH



NORTH ELEVATION



WEST ELEVATION

REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	

LP3 architecture
 158 N. GLASSSELL ST., STE. 201
 ORANGE, CA 92866
 714-771-8400

OLD TOWNE GATEWAY
 401 WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

SCALE: 1/8" = 1'-0"

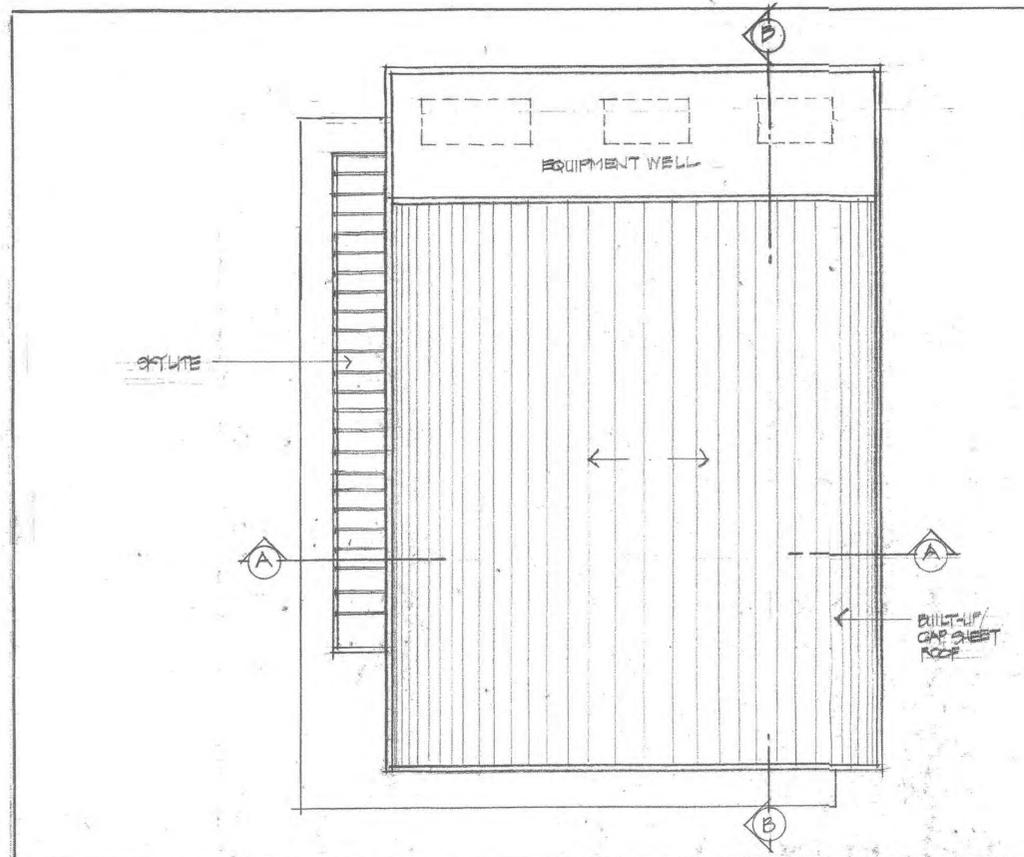
DRAWN: LPP III

DATE:

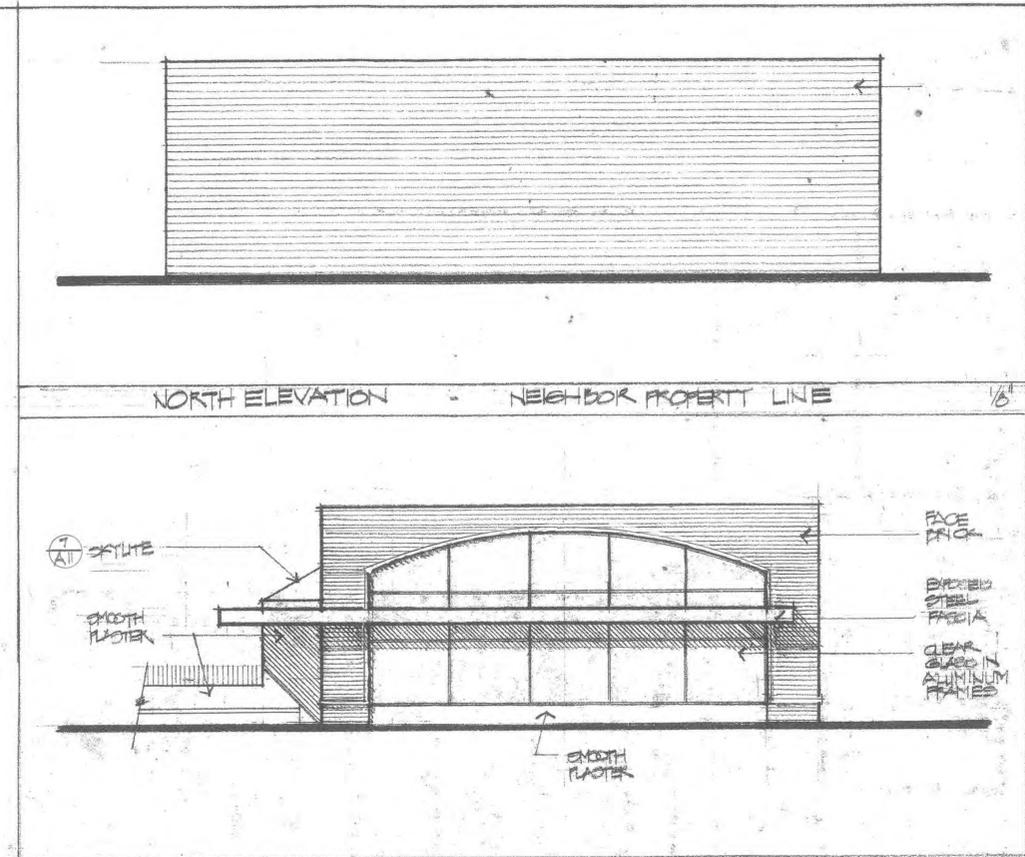
BUILDING 2
 PLANS/SECTIONS
 ELEVATIONS

SHEET NO.

A9



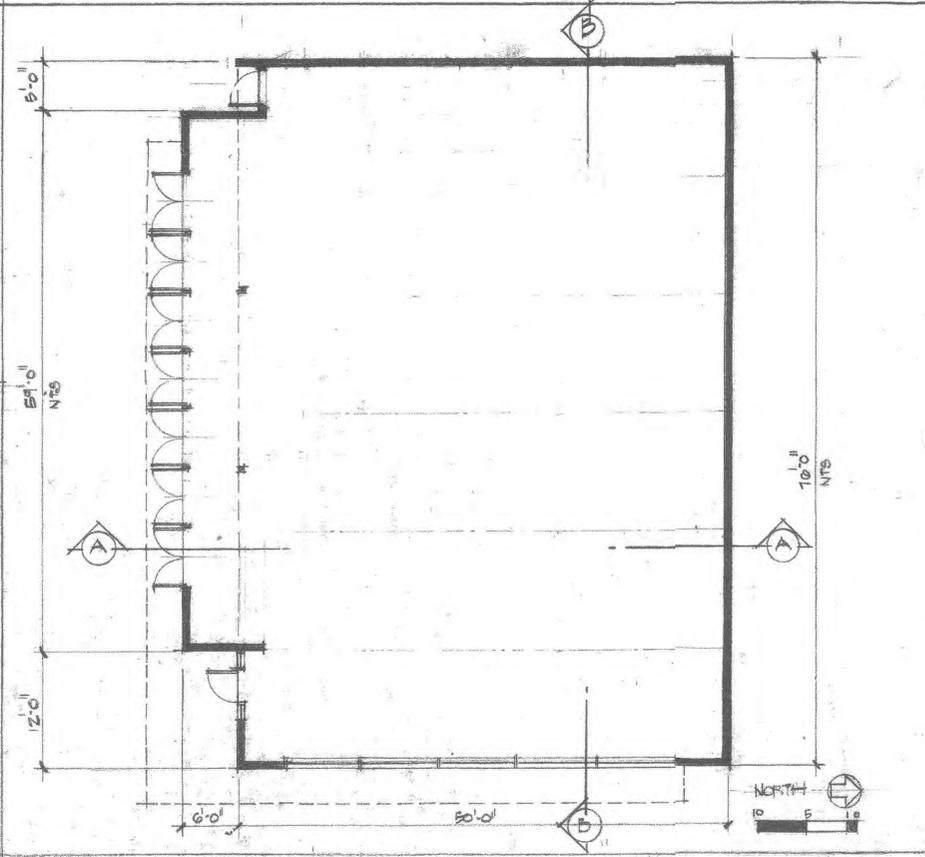
ROOF PLAN



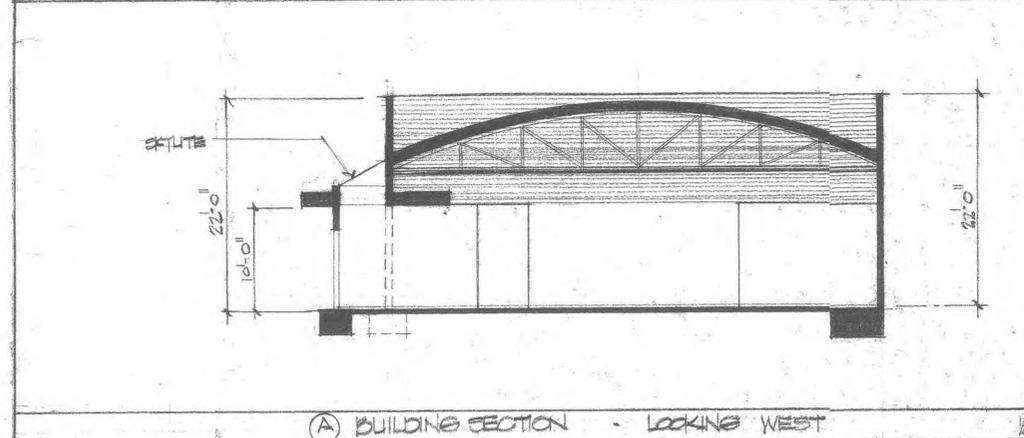
NORTH ELEVATION - NEIGHBOR PROPERTY LINE



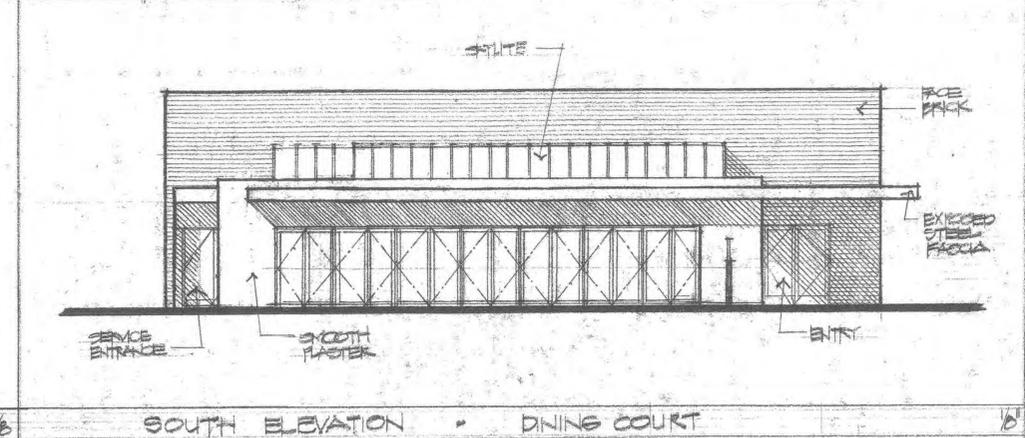
EAST ELEVATION - CYPRESS STREET



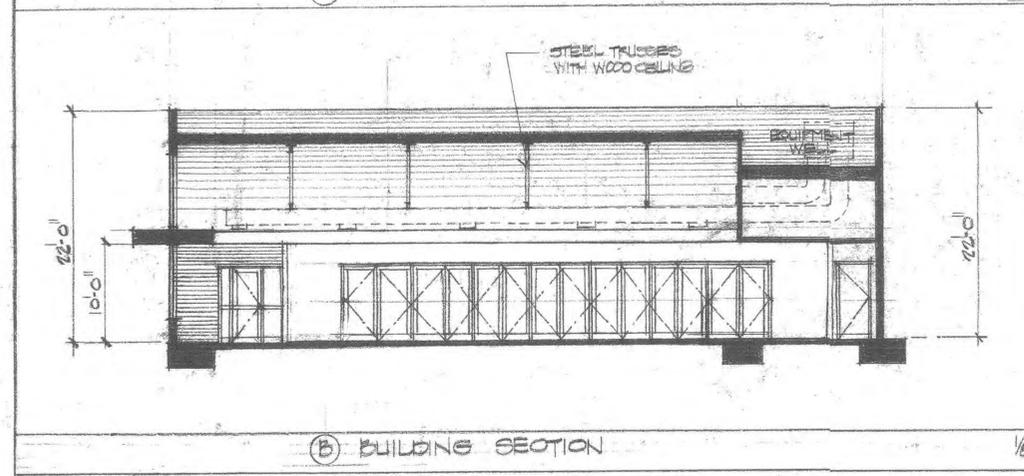
FLOOR PLAN



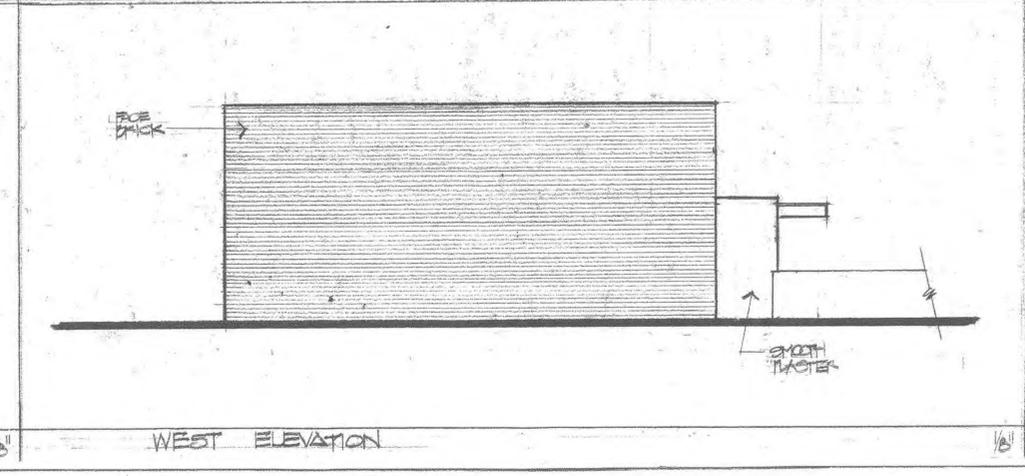
BUILDING SECTION - LOOKING WEST



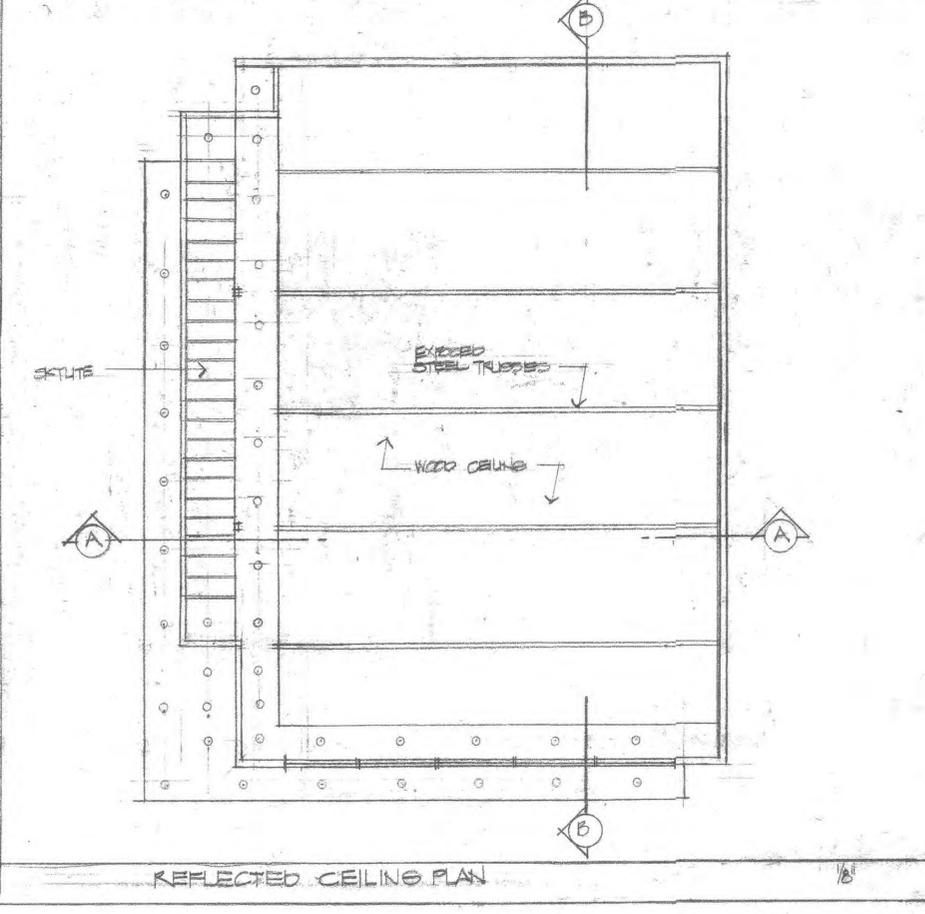
SOUTH ELEVATION - DINING COURT



BUILDING SECTION



WEST ELEVATION



REFLECTED CEILING PLAN

REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	

LP3 architecture
 158 N. GLASSSELL ST. STE. 201
 ORANGE, CA 92866
 714-771-8400

OLD TOWNE GATEWAY
 401 WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

SCALE: 1/8" = 1'-0"
 DRAWN: LPP III
 DATE:
 BUILDING 3
 PLANS/SECTIONS
 ELEVATIONS
 SHEET NO.
A10

REVISIONS	
NO.	DATE
1	
2	
3	
4	
5	
6	

LPS architecture
 158 N. GLASSELL ST. STE. 201
 ORANGE, CA 92866
 714-771-8400

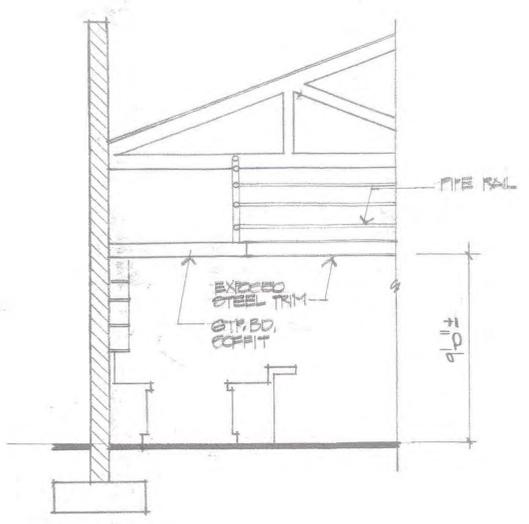
OLD TOWNE GATEWAY
 401 WEST CHAPMAN AVENUE
 ORANGE, CALIFORNIA 92866

SCALE: AS NOTED
 DRAWN: LPS III
 DATE:
 HISTORIC BUILDING 4
 BUILDING 5
 ATCHESON ELEVATION
 SHEET NO.

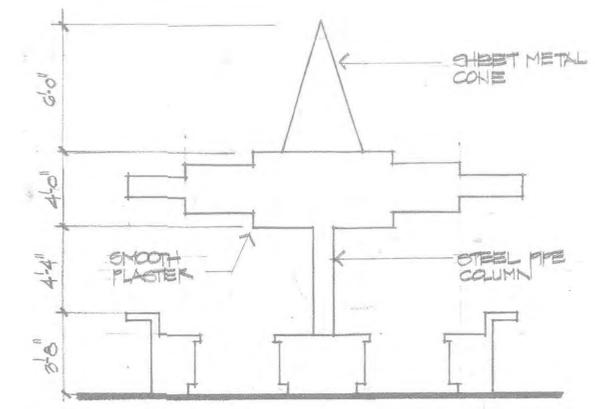
A11



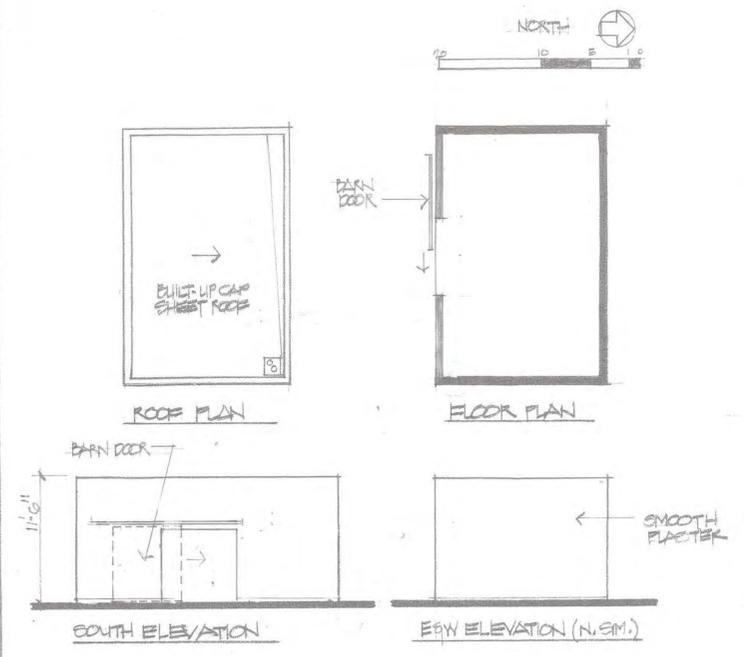
ARCHITECT STREET ELEVATION



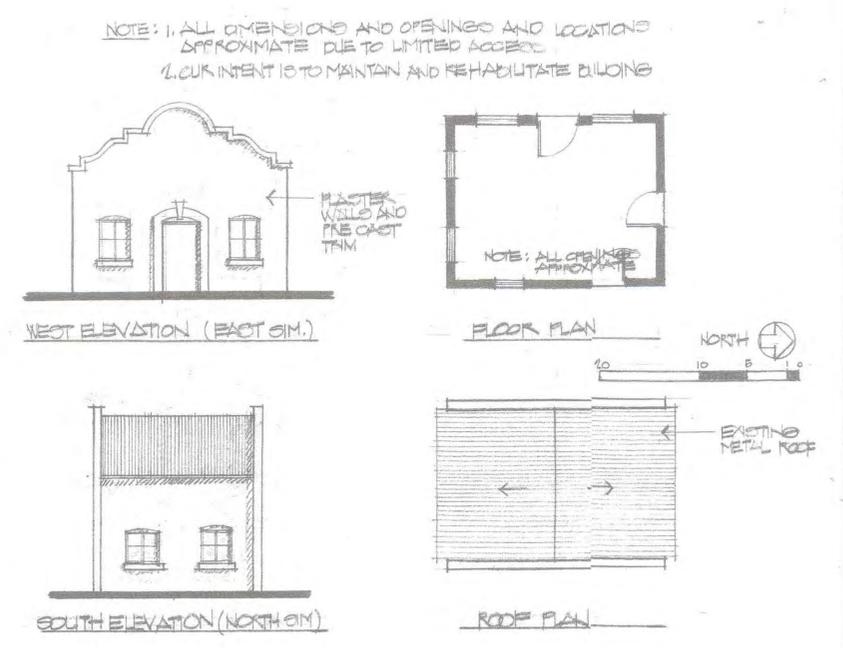
1 DETAIL AT BAR "CAT WALK" 1/4"



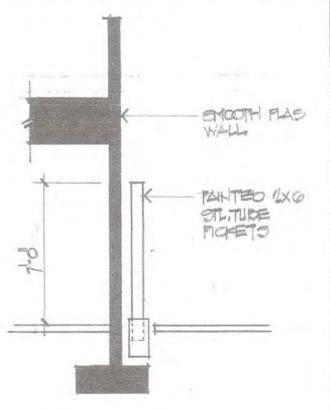
2 DETAIL AT OUTSIDE BAR 1/4"



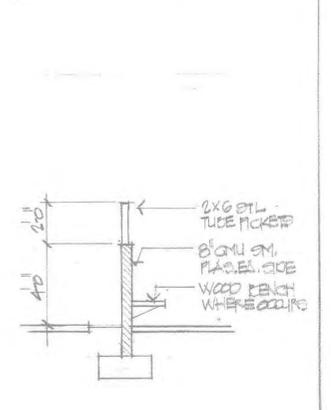
4 BUILDING 5 1/8"



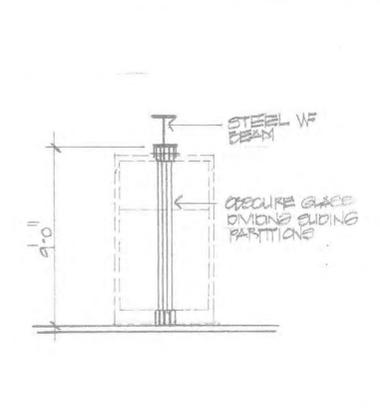
4 BUILDING 4 1/8"



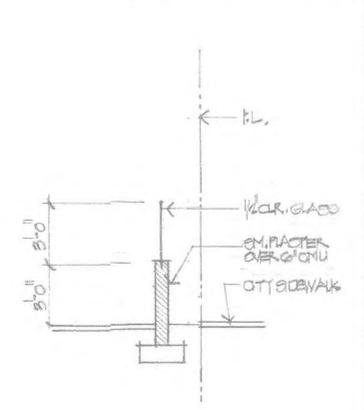
6 PAINTED PICKETS 1/4"



5 SECURITY FENCE 1/4"



4 GLASS SLIDING PARTITION 1/4"



3 SECURITY FENCE AT FL. 1/4"